



The SOUTHERN MERCANTILE Style is an interesting architectural variation in its simple expressive forms balanced with sometimes intricate and always interesting textural detail. It might be better termed as ‘Main Street Vernacular’ since it is a building style you may find on main streets in all sizes of towns and cities across the country. As in all national styles there are common elements that create the framework for the local vernacular to graft onto and adapt to their own particular traditions or uses.

Southern Mercantile buildings address all the practical needs for the building occupants and its visitors. They range in size from the one-store one-story small shops of individual merchants all the way to the 3 to 4 story mill buildings that housed the manufacture or storage of the goods to be sold on Main Street. Large glazed openings of ganged windows and doors maximize the view of goods for sale to passers-by and provide generous light for the interior as many buildings only have light from one wall.

Upon this utilitarian need for visibility the wonderful creativity of brickwork and wood trim satisfied the more ephemeral need for visual interest, distinction, and beauty. A Southern Mercantile building celebrates its status on the street as a handsome accompaniment to its neighbors.

APPROPRIATE BUILDING TYPOLOGIES

- Street-Front Buildings
- Grocery Building
- Parking Structure

IDENTIFYING FEATURES

1. MASONRY WALL CONSTRUCTION
2. FLAT ROOF WITH ORNAMENTED PARAPET
3. PUNCHED OPENINGS AT UPPER FLOORS WITH HIGHER CONCENTRATION OF GLAZING AT LOWER LEVEL
4. BRICK AND MASONRY DETAILING (I.E. CORBELING, BANDING, ARCHES, OR DECORATIVE MOTIFS)
5. HORIZONTAL AND VERTICAL BANDING SUGGESTIVE OF POST AND LINTEL STRUCTURE
6. LARGE GLAZED STOREFRONT ON STREET

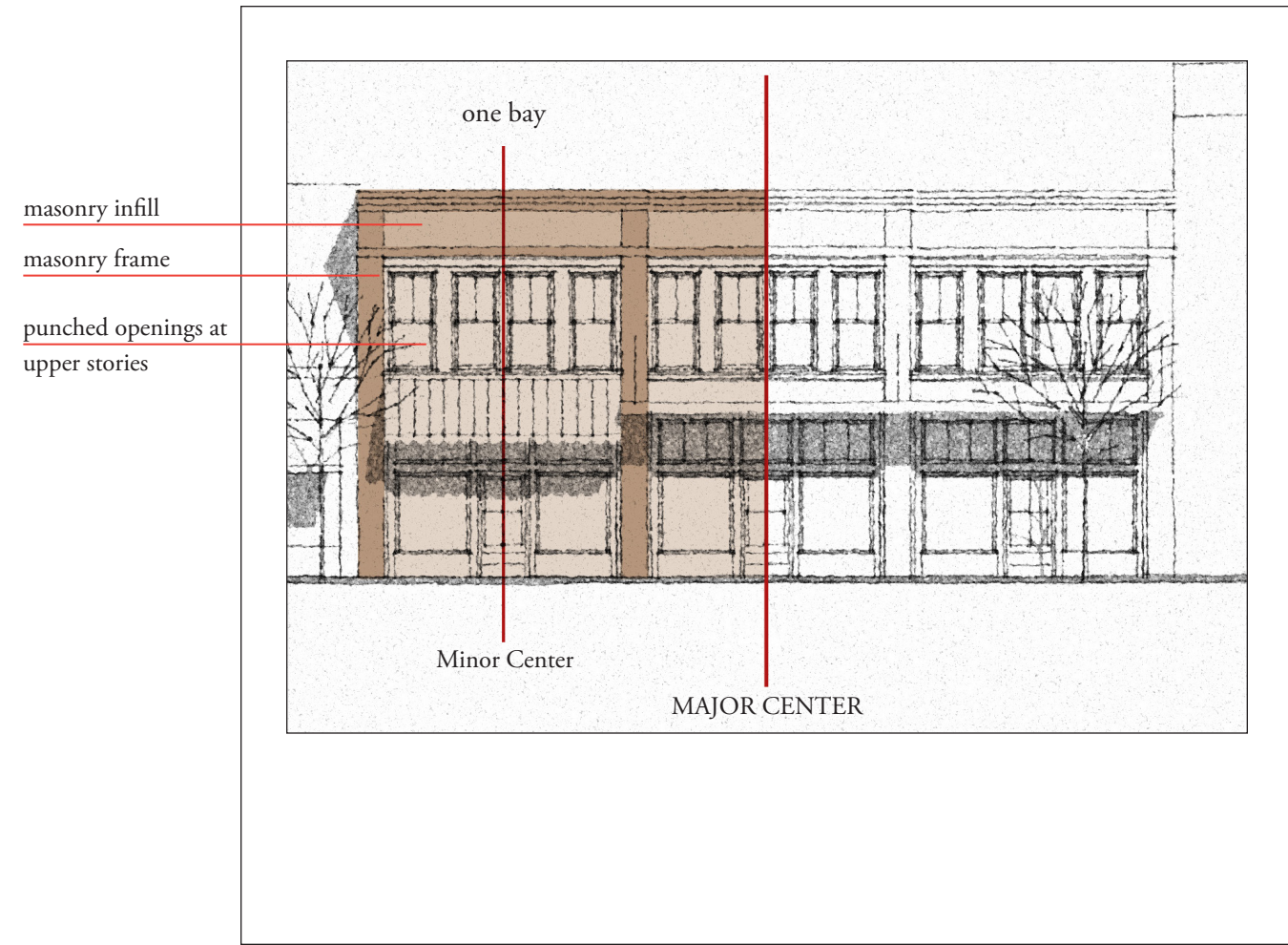
APPROPRIATE BUILDING MATERIALS

- Wall materials: brick, stone, stucco, painted brick
- Trim/accent materials: brick, cast stone, wood
- Roofing material: metal, flat clay or concrete tiles, composite architectural shingles

ADDITIONAL RESOURCES

- *Designs on Birmingham* edited by Philip Morris and Marjorie White, published by the Birmingham Historical Society.
- *Mountain Brook Village: Then & Now* by Linda Nelson and Marjorie White, published by the Birmingham Historical Society.
- *The Abrams Guide to American House Styles* by William Morgan.
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SOUTHERN MERCANTILE DESIGN CONCEPTS

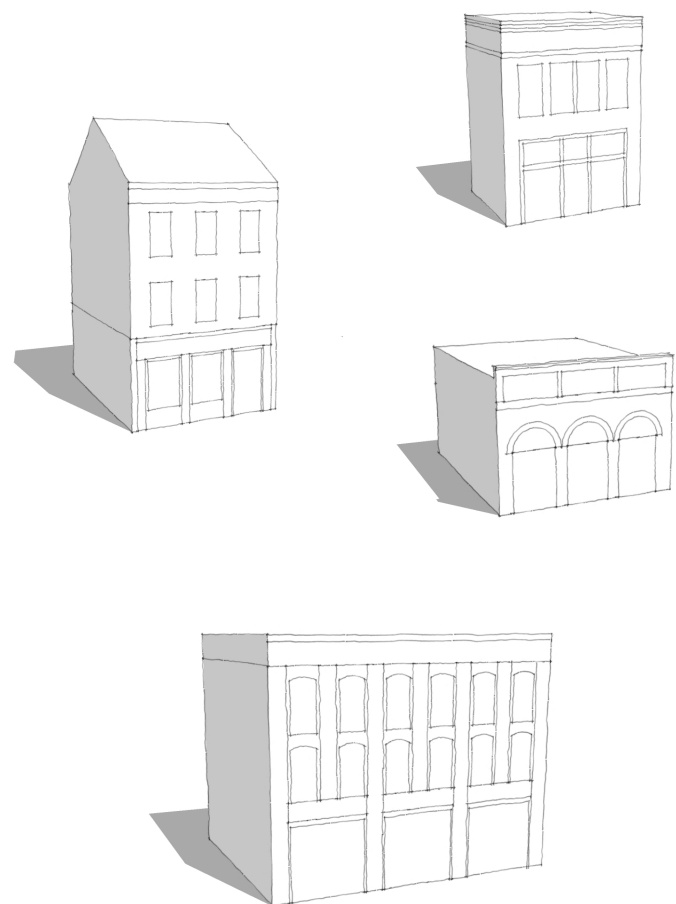
SYMMETRY - A Southern Mercantile building will have a structural order guided by the width of the overall building and its division into smaller bays. There is a freedom in the choice and the number of bays which are only limited by the practical requirements of the structure and the overall length of the property. Common bay numbers are 1, 2, 3, 4, & 5.

PROPORTION - Southern Mercantile proportions are based upon and generated from very simple formulas and are very responsive to utilitarian necessities (structural spans, column heights, etc). Wall opening dimensions need to honor standard masonry unit sizes, minimizing awkward material cuts and misaligned openings. Building corners should maintain a 24" solid surface before the introduction of any glazing to better 'carry' the building. The overall goal of the style should be an appearance of solid simplicity.

SURFACE - A Southern Mercantile surface is well detailed. Openings are recessed deeply, a minimum of 8", and parapets can project as much as 36" with transitional depths or visual supports building up from 0 (wall plane) to 36 (edge of cornice). The use of shade and shadow is very important to the reading of a Southern Mercantile building. Simple, ornamental brickwork is a way for the store-owners to differentiate themselves from their neighbors while being practical and using very similar construction methods and dimensions. If handled with attention and care and based upon historic examples, this design style can be very pleasing to the eye.

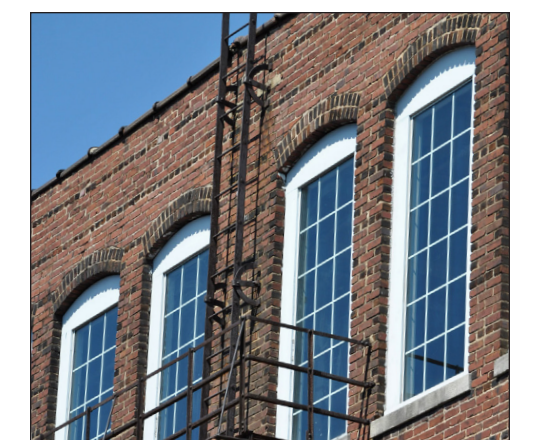
HIERARCHY OF SCALE - Masonry details are more prevalent in this style more so than any others, which may take more study and care to execute successfully. Any and all efforts exerted in this area will result in a more valuable and attractive structure.

SOUTHERN MERCANTILE MASSING EXAMPLES



Southern Mercantile building openings are one of two types - punched or infill. Punched openings typically occur in the upper stories of mercantile buildings, while the infill is usually used in the ground floor to increase glazing area. The punched openings are large in order to maximize the penetration of light into the building interior. Punched openings can be handled in a variety of ways, but all should be recessed from the wall by at least 4".

The primary design intent is to be functional yet simply adorned in a way that expresses the structural components of the elevation.





The parapet of a Southern Mercantile building is where you will see the greatest expenditure of design creativity in this style - an amazing variety can be achieved through simple manipulations of a small, standard building component called the brick. A brick's plain rectangular shape on its own does not immediately equate with beauty or complexity, but a simple glance at the accompanying images will expose its wonderful possibilities.

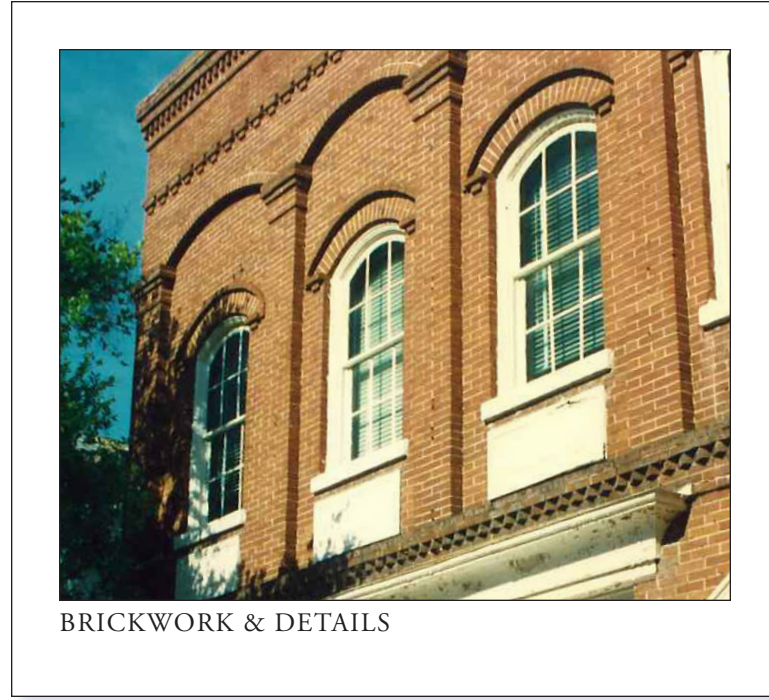
Southern Mercantile parapets can be as simple as a single row of offset brick in the same coursing as the wall below or as elaborate as the one illustrated below and be a mixture of ornamental sheet metal or wood cornice detailing and brick surrounds. Wall coping is typically natural or cast stone 6"-8" thick. Heights range from 24"-48".



Southern Mercantile Storefronts are large and can either be infill between masonry piers or punched into the wall plane similar to upper story windows in this style. The storefronts are typically larger in width than height and they contain a majority of glazing punctuated by the entry doors to the interior. Wood or Metal framing up to 12" in width provide the framework for the window sash and doors, which can be a combination of both large and small panes. Although there are many possible successful solutions, the main concepts to maintain are pane proportions between windows and doors and that as one rises in elevation the panes typically decrease in size. Metal columns and entablature can add another layer of scale to the building but are not required. Within the general rectangular openings the entry doors may be placed in any desired composition as symmetry at this smaller scale is less necessary for success - often the entry bay is recessed further into the building than the surrounding windows to create a covered area and to accentuate the entry. Entries should be larger than normal, often exceeding one door in width, and entry doors should be at a minimum 50% glazed. Other building bays may have additional entry points but the main entry should always be clear to the pedestrian.

The simplest way to accomplish this clarity is by placing the store signage directly over the main entry point, either on the awning or applied directly to the wall or entablature above the entry. Larger scale signage to be viewed from afar is best located on the parapet wall surface.





BRICKWORK & DETAILS

Southern Mercantile brickwork and accent masonry details are what differentiate it from the more austere Village Mid-Century style. In general the offsets and angles are exaggerated to create bold, vibrant shadows that enliven the overall building facade. The primary locations for this type of detail to occur is on the parapet, at the window heads, and between the storefront and upper story windows.



AWNINGS & MISCELLANEOUS

Southern Mercantile awnings can range from flat metal planes held out from the wall surface by links or chains to soft, striped, curved canopies that move in the breeze. While the rectangular metal roof might seem too similar to the Birmingham Classic, at the smaller scale there will be more articulation at the surfaces of a Southern Mercantile metal awning.

There are no hard and fast typical rules for such a variety of design possibilities, hard or soft, shallow or deep, low or high pitch, sign or no sign; but the awning should relate to and enhance to overall building composition.





The BIRMINGHAM CLASSIC architectural style encompasses elements from Federal, Georgian, and Classical Revival styles inspired by English and Colonial precedent. The classical buildings found in and around Birmingham share the core characteristics of the classical tradition, but express subtle differences from similar styles in other parts of the South. This style is found in many public buildings, college campuses, and stately homes around Birmingham.

The dominant feature of these buildings is the simple and symmetrical massing embellished by more refined classical ornamentation. Roof forms are typically gabled or hipped with slopes no greater than 6:12. In most cases these buildings are constructed with brick and/or stone masonry with the higher style examples using cast stone for their ornamentation. More modest examples are constructed of brick masonry with wood ornamentation. The entries and doors are often intricately detailed to bring a pedestrian scale to some of the larger structures.

APPROPRIATE BUILDING TYPOLOGIES

- Street-Front Buildings
- Free Standing
- Residential
- Inn

IDENTIFYING FEATURES

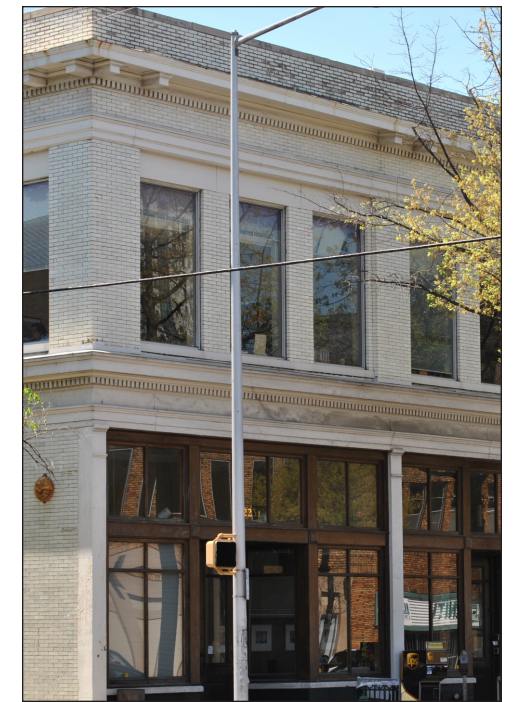
1. SIMPLE VOLUMES ADDITIVE PORCHES OR SMALLER WINGS TO MAKE MORE COMPLEX SHAPES. RECESSED COLONNADES ARE ALSO COMMON.
2. SYMMETRICAL COMPOSITION OF DOORS AND WINDOWS
3. SIMPLIFIED CLASSICAL DETAILING AT ENTRIES, WINDOWS, CORNICES AND WALLS
4. ROOF PITCHES TYPICALLY 6:12 OR LESS FOR THE MAIN MASSING WITH SHALLOWER PITCHES (3-5:12) FOR FRONTAL TRIANGULAR GABLES OR SECONDARY MASSES AND PORCHES

APPROPRIATE BUILDING MATERIALS

- Wall materials: brick, painted brick, stucco, and stone masonry
- Trim/accent materials: brick, cutstone, cast stone, and wood
- Roofing materials: slate, flat clay or concrete tiles, metal or composite architectural shingles

ADDITIONAL RESOURCES

- *Designs on Birmingham* edited by Philip Morris and Marjorie White, published by the Birmingham Historical Society.
- *Mountain Brook Village: Then & Now* by Linda Nelson and Marjorie White, published by the Birmingham Historical Society.
- *The Abrams Guide to American House Styles* by William Morgan.
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BIRMINGHAM CLASSIC DESIGN CONCEPTS

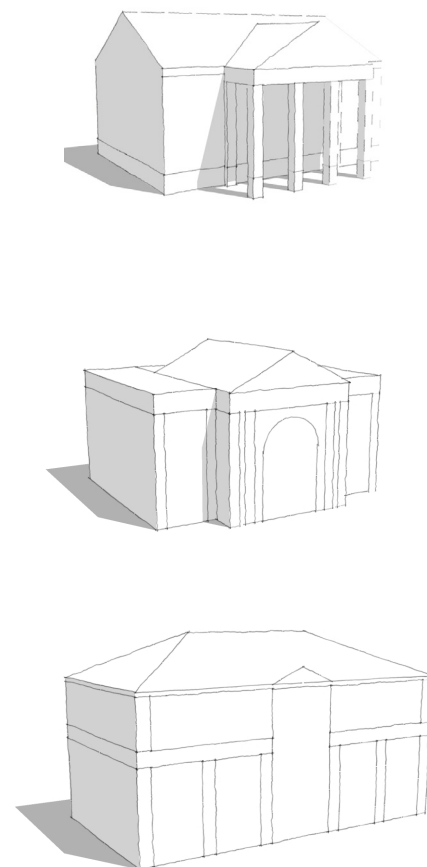
SYMMETRY - The most prominent feature of a Birmingham Classic building will be its bilateral symmetry, each side generally matching the other along the imaginary centerline. This symmetry mimics the natural 'design' of animals and humans that is generally symmetrical.

PROPORTION - The organization of a Birmingham Classic building is based upon and generated from proportions which can be found in nature and many historic structures. The classical canons of Tuscan, Doric, Ionic, and Corinthian proportions can be applied to appropriately size cornices, columns, openings, pedestals and bases. Typical ratios include 1:1, 1:2, 1:3, 2:3, 3:5, 1:1.618 (the Golden Section)

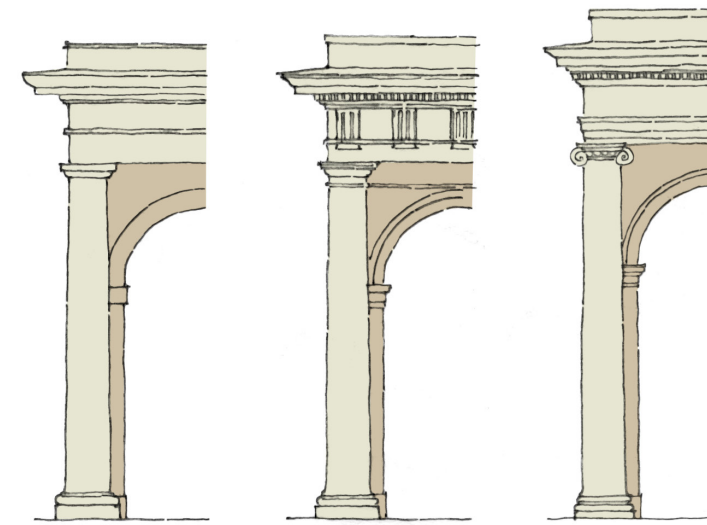
SURFACE - Monumental Birmingham Classic buildings should be constructed of lasting materials such as brick, stone, or stucco. Buildings that are more commercial in nature may have wood trim, and buildings more residential in nature may be constructed of primarily wood. Openings should be recessed into masonry walls at a minimum of 2".

HIERARCHY OF SCALE - A Birmingham Classic building can be a stand-alone monument or part of the urban fabric. It includes large scale elements such as porticoes which can be experienced from a distant urban scale and small scale elements such as mouldings to relate to the smaller human scale. However, all sizes of details require the use of appropriate proportions to compose an aesthetically pleasing and successful building.

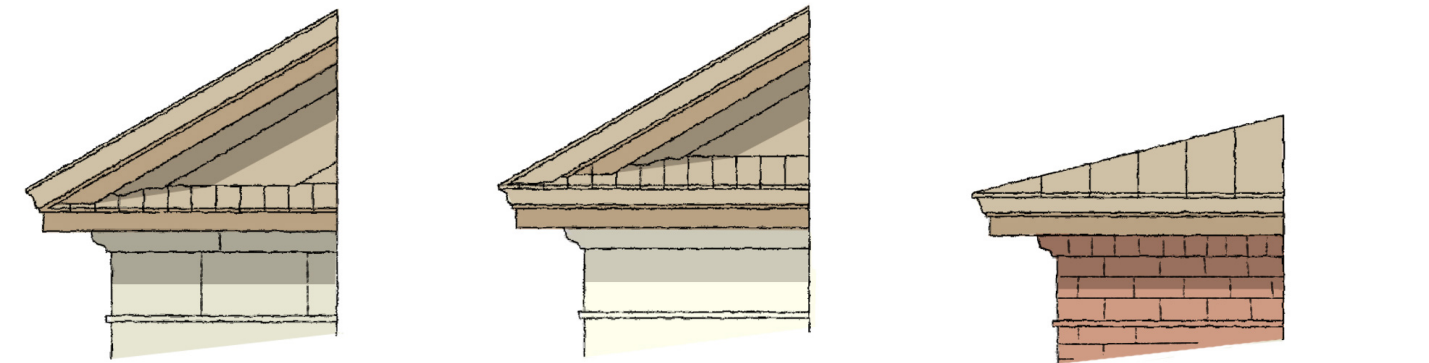
BIRMINGHAM CLASSIC MASSING EXAMPLES



Birmingham Classic buildings draw heavily on classical details for embellishments at important portals and horizontal planes. The profiles and proportions of these details are dictated by the classical tradition that is well documented. The most prominent proportional systems are expressed in the classical canonical orders of Tuscan, Doric, Ionic, and Corinthian. More information about these traditions can be found in additional resources such as *The American Vignola* by William Ware or *Parallels of the Classical Orders of Architecture* by Normand and von Mauch.



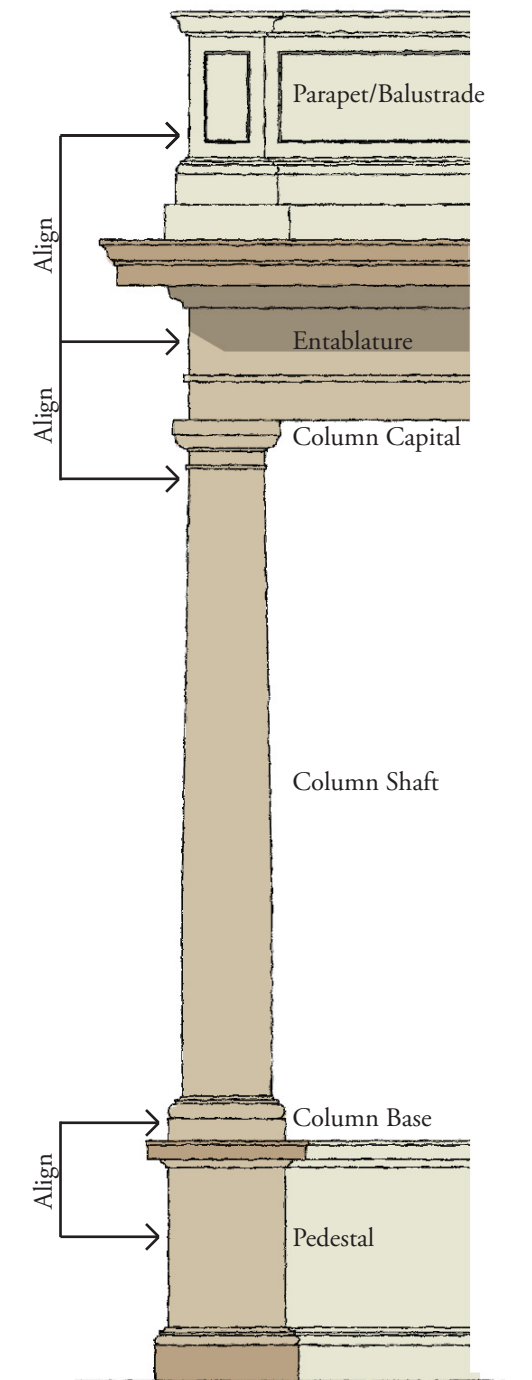
Arch Conditions - Tuscan, Doric, Ionic (left to right)



Gable with Stone Cornice - High Style

Gable with Wood Cornice - Mid-Style

Classical Brick Cornice with Hip Roof





ENTRY FEATURES

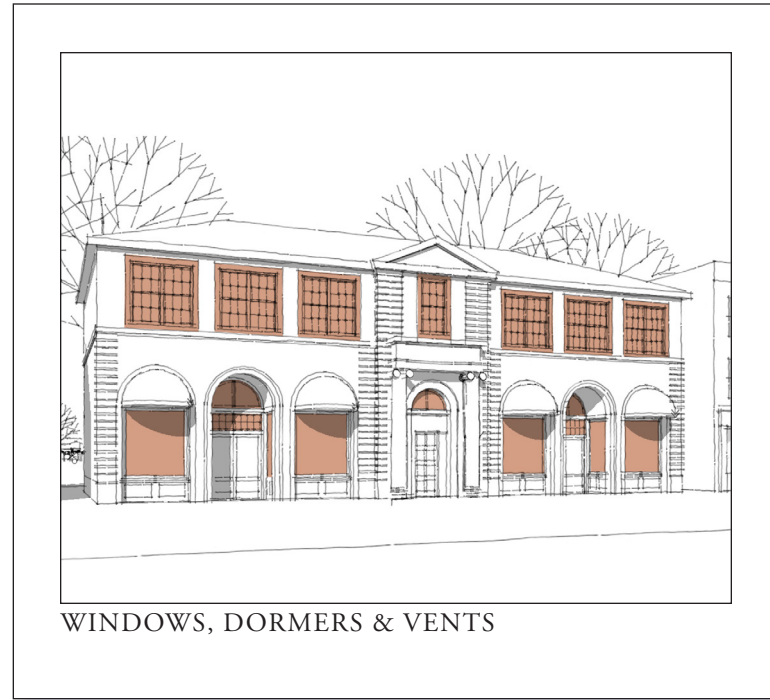
The entries into buildings of the Birmingham Classic style are the most articulated elements on the facade. They are highlighted with ornamental embellishments and architectural features. The entry most often occupies the central bay on a bilaterally symmetrical facade. Specifically on public buildings, there is often a large loggia or arcade that provides shelter and a place for people to gather. These arcades feature an odd number of bays, with the entry occupying the central bay. Retail buildings may have a flat expression of an arcade filled with glass as their storefront windows. Storefronts can also take the form of large trabeated openings with an articulation of small pilasters and mouldings around the glass within the large masonry opening.



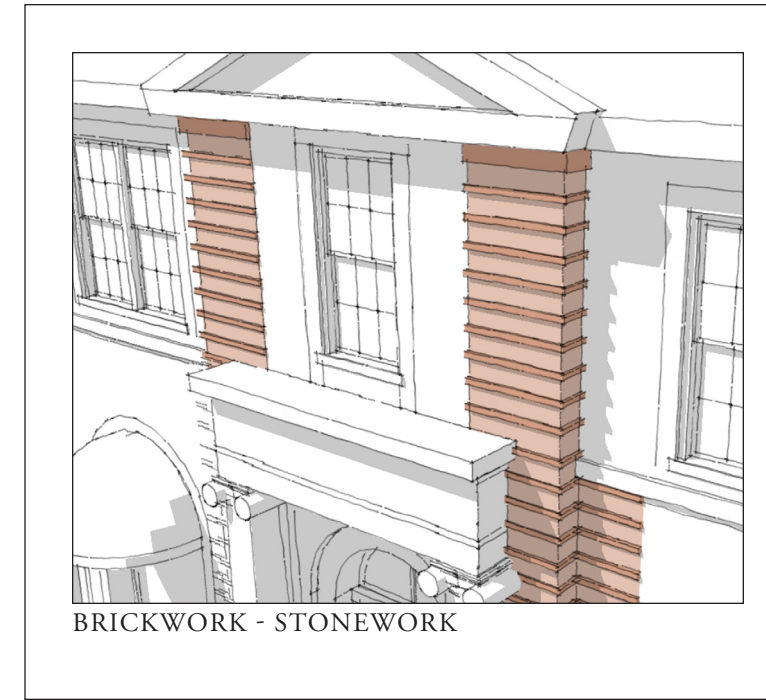
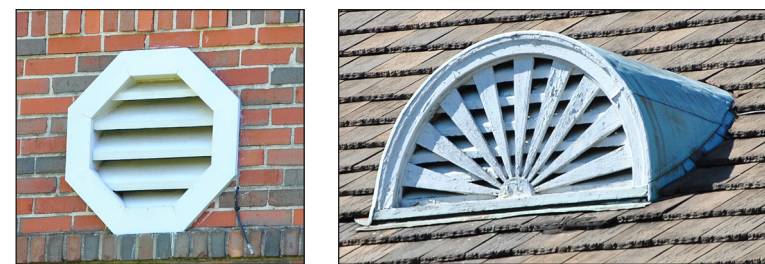
DOORS

The main door of a Birmingham Classic building is often highly embellished and the central element on the facade. In these instances, the entry takes the form of large double doors with a rectangular or semicircular transom. On less monumental buildings, the entry takes the form of a single door with sidelights. In both cases, the door surround is articulated with a portico, pilasters or columns. Less important side doors, or residential entries often display a stripped down version of columns or pilasters with a shallow gable or bracketed shed roof similar to an awning.





Windows on Birmingham Classic buildings shall be vertical in proportion, though several windows may be ganged in order to form a more horizontal composition. Windows may have a half-round or elliptical head. Windows should be operable with double-hung or casements preferred. Standard minimum window size shall be 2'-8" in width and 5'-0" in height. Window head heights should align at each story of the building. Dormers, where appropriate, should be constructed of wood and not masonry as the main body of the structure. Small accent windows in non-rectangular shapes are allowed for accents such as in the pediment of a facade. Windows should be articulated as punched openings with inset wood trim. The masonry opening may be articulated as a round, elliptical, or jack arch. Shutters may be used, but must be sized appropriately to the opening and have functioning hardware and hinges.



Brick or stone may be used to create texture on the facades of Birmingham Classic buildings. Contrasting brick or stone may be used in horizontal bands to imply a base, watertable, or cornice. Quoining may be implied at the corners of buildings, surrounding large openings, or below the edges of a pediment to imply pilasters. Contrasting or protruding brick or stone may be used to highlight architectural features or imply a bay rhythm where no glazing is present.





The VILLAGE MID-CENTURY Architectural Style is typified by an early to mid-century modern aesthetic of machine-like craft and the absence of ornament. It is important to consider that while not as architecturally honoured as the more typical historic styles, done well, it is still valid and pleasing to both users and viewers. Many practitioners of this style were trained in the traditional schools of architecture where proper relational proportions are highly valued. This means that whatever the design goal, the relationships between building height and width, and the building to its elements were always considered as a coherent composition.

In a general view the Village Mid-Century building appearance is horizontal. It is a smaller scale 1-2.5 story building which can be expanded horizontally to accommodate more utilitarian building types such as grocery stores or restaurants, where you want large openings to engage customers or for al fresco dining. The Village Mid-Century is a local designation appropriate for Mountain Brook Village in that many of the buildings in the existing Village developed in the 50's and 60's take this style.

APPROPRIATE BUILDING TYPOLOGIES

- Street-Front Buildings
- Free Standing
- Parking Structure
- Grocery Building

IDENTIFYING FEATURES

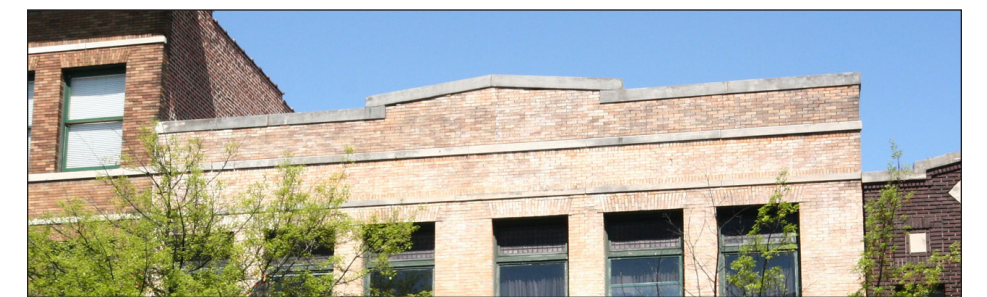
1. LARGE PUNCHED OPENINGS IN MASONRY WALLS
2. SIMPLE, MINIMAL SURFACE ARTICULATION
3. HORIZONTAL PROPORTIONS IN MAJORITY OF ARCHITECTURAL DESIGN ELEMENTS
4. METAL WINDOWS WITH MULTIPLE PANES AND AWNING OPENINGS
5. PARAPET WITH SIMPLE COPING THAT IS OFTEN STEPPED OR SLOPED IN HEIGHT

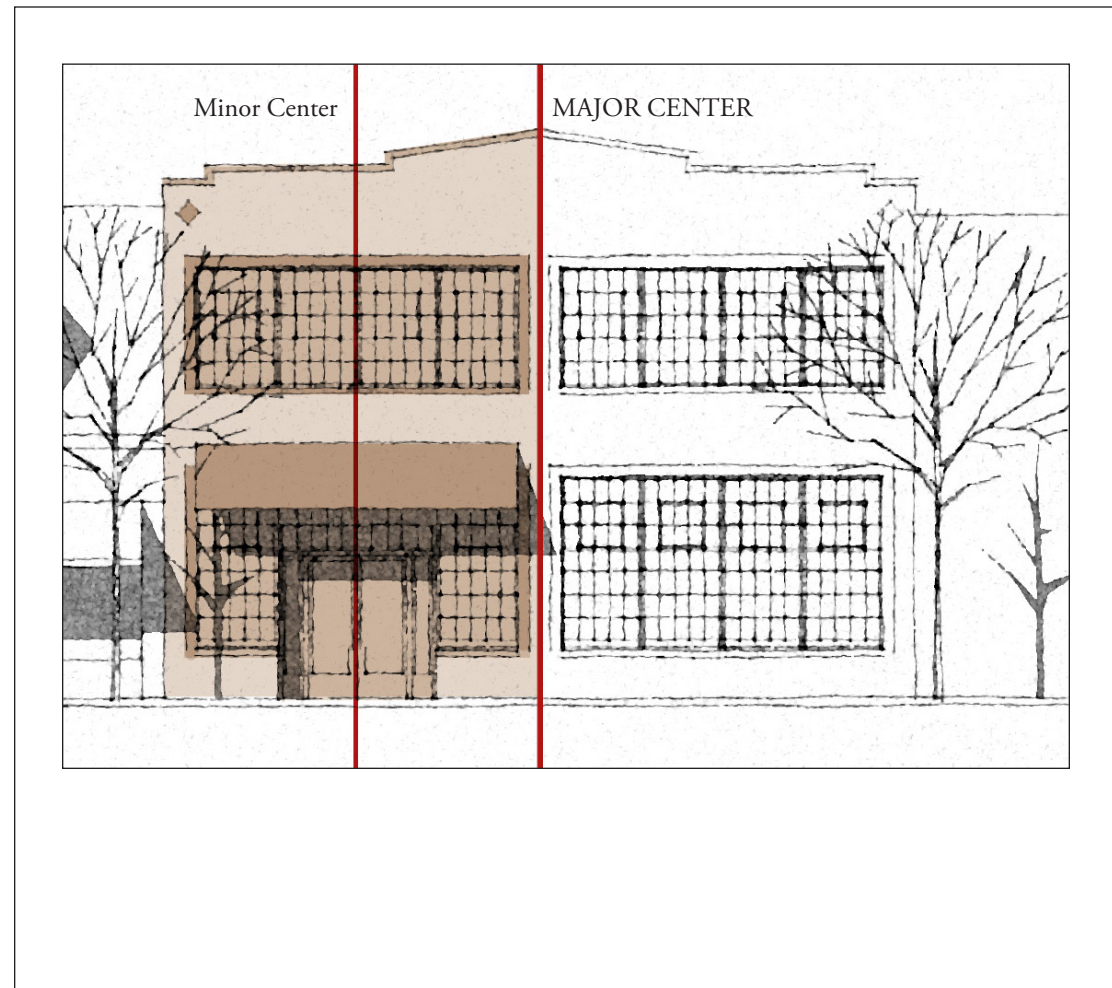
APPROPRIATE BUILDING MATERIALS

- Wall material: brick, painted brick, stucco, cut or cast stone
- Accent material: brick, stone, cast concrete, metal
- Roofing material: metal, flat clay or concrete tiles, composite architectural shingles

ADDITIONAL RESOURCES

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VILLAGE MID-CENTURY DESIGN CONCEPTS

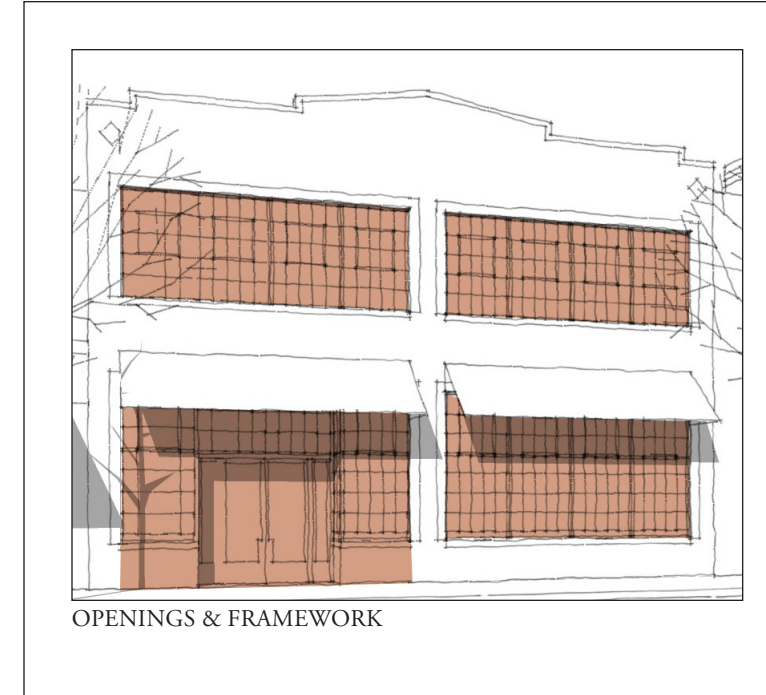
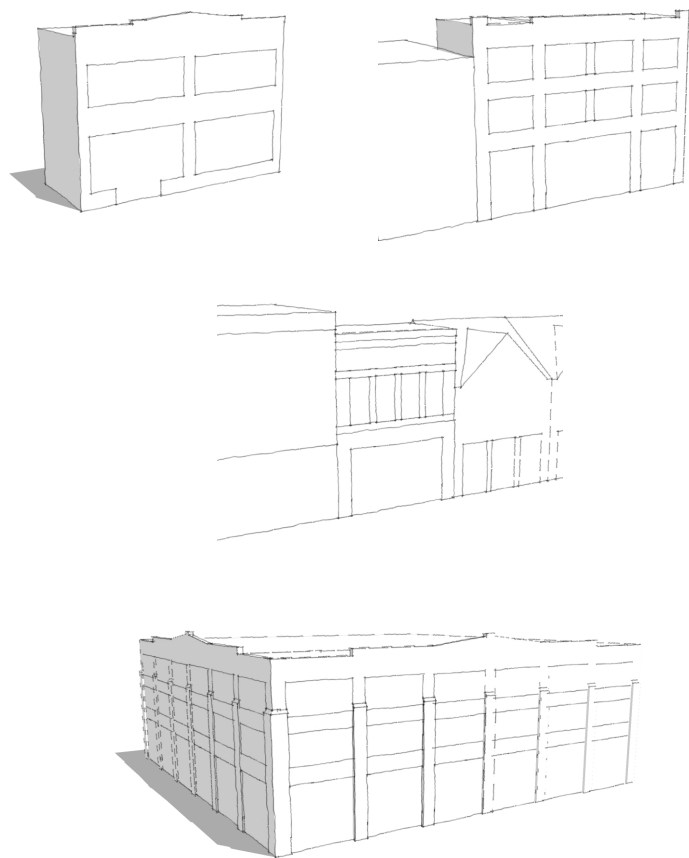
SYMMETRY - A Village Mid-Century building is very similar to the structural symmetry of the Southern Mercantile style and will also have a visual order guided by the width of the overall building and its division into smaller bays. There is a freedom in the choice and the number of bays which are only limited by the practical requirements of the structure and the overall length of the property. Common bay numbers are 1, 2, 3, 4, & 5.

PROPORTION - Village Mid-Century proportions are based upon and generated from very simple formulas and are very responsive to utilitarian necessities (structural spans, column heights, etc). Wall opening dimensions need to honor standard masonry unit sizes, minimizing awkward material cuts and misaligned openings. Building corners should maintain a 24" solid surface before the introduction of any glazing to better 'carry' the building. The overall goal of the style should be an appearance of solid simplicity.

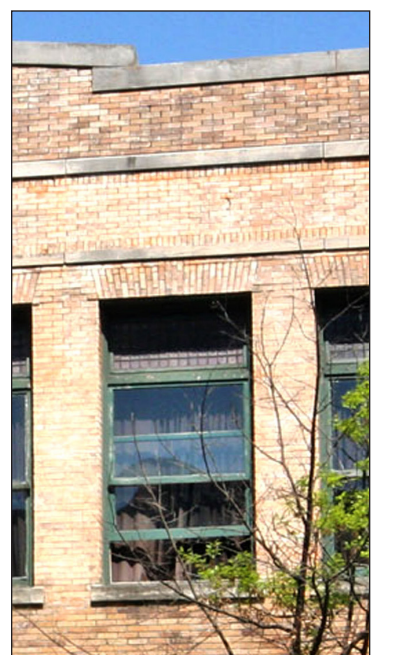
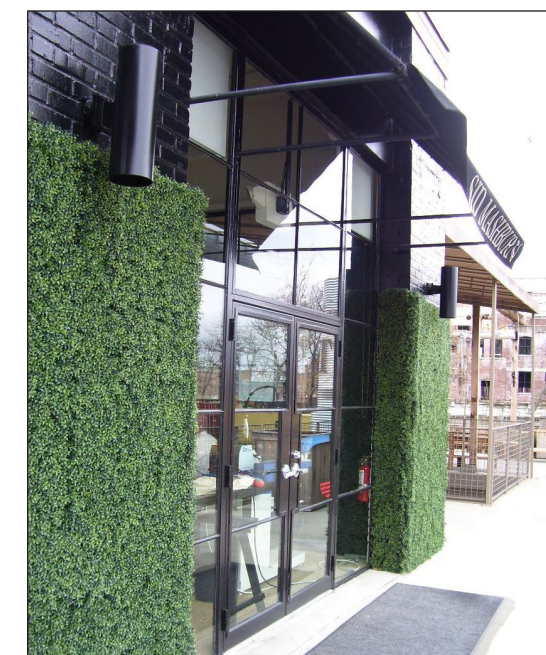
SURFACE - A Village Mid-Century surface is meant to be 'clean'. Openings are not recessed deeply, to a maximum of 8". Any protrusions from the wall plane do not project more than 4-6" typically. These dimension dictums reduce the visual depth of the building by reducing the shadows that are cast by the sun. If handled with attention this effect can be very aesthetically pleasing.

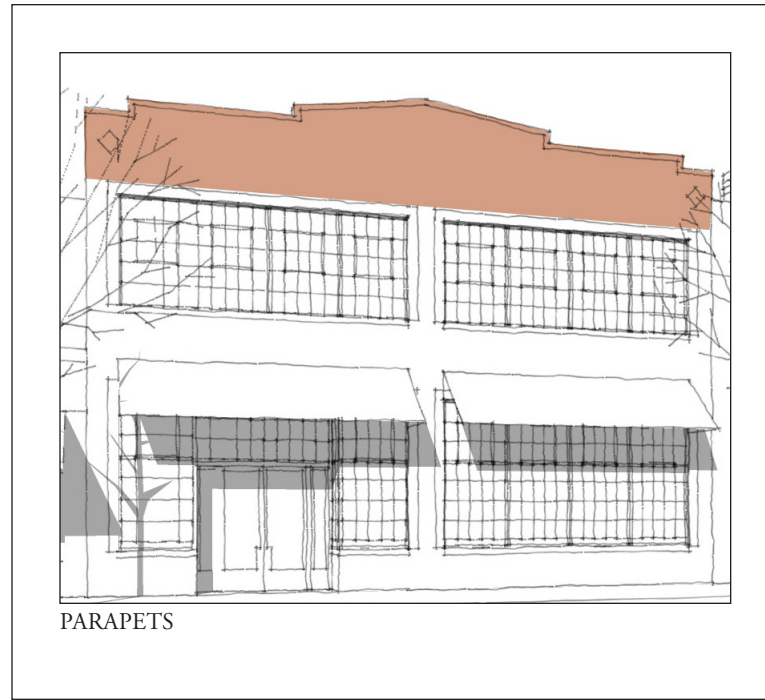
HIERARCHY OF SCALE - Details are less prevalent in this style more so than any others, but that does not indicate or mean that concern for the human scale is absent or ignored. It is actually more important here that these smaller scale details be correct because there are so few of them.

VILLAGE MID-CENTURY MASSING EXAMPLES



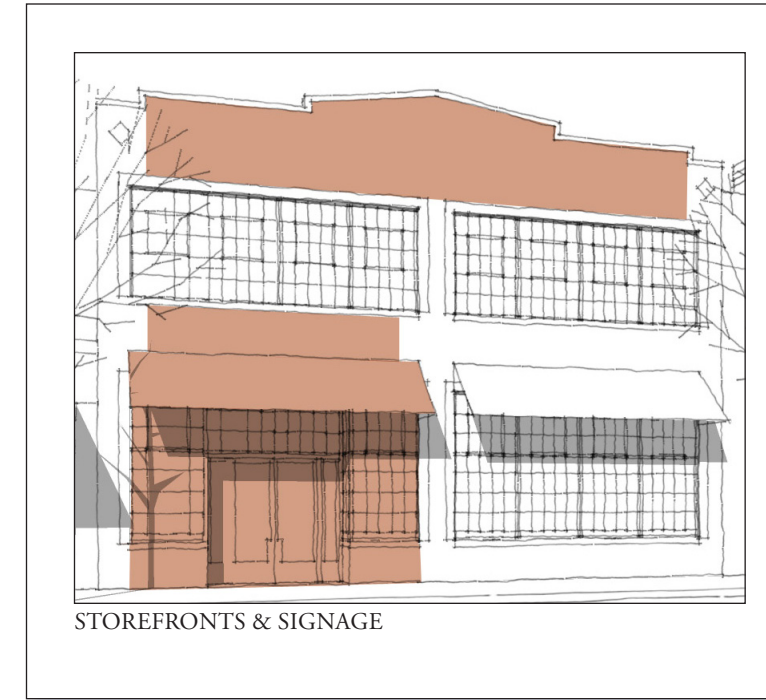
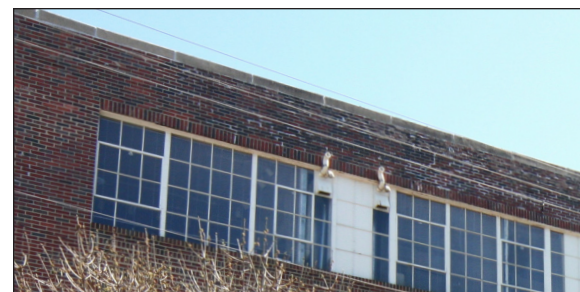
Village Mid-Century building openings are very large and horizontal in proportion. They are created by placing duplicate or similar windows next to each other in groups of two, three, four, and five. The primary design intent is to express a clean and horizontal aesthetic with minimal ornamental pattern and detail. Brickwork detailing may or may not be utilized at the edge of the openings - if it is it is generally 8-12" of brick and/or other masonry. Simplicity is paramount. Windows will be metal with multiple panes and thin frame profiles. See Storefronts and Signage for details on that respective feature.





Village Mid-Century parapets are often the architectural feature that gets the most attention. At its simplest the lone detail is a stone or tile coping that presents a contrasting band of color, emphasizing the buildings mass horizontally against the sky. Vertical steps are introduced to add architectural interest to the corners or center or to draw attention to signage or other elements like window bays.

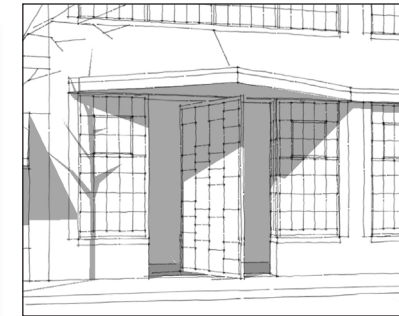
In all cases the integrity of the planar quality of the wall surface is to be maintained. In rare occurrences if ornament is desired the motifs of Art Deco parapet sculpture would be most appropriate due to its shallow relief which similarly minimizes the strong contrast of sunlight and deep shadow while still seeking to individualize designs.

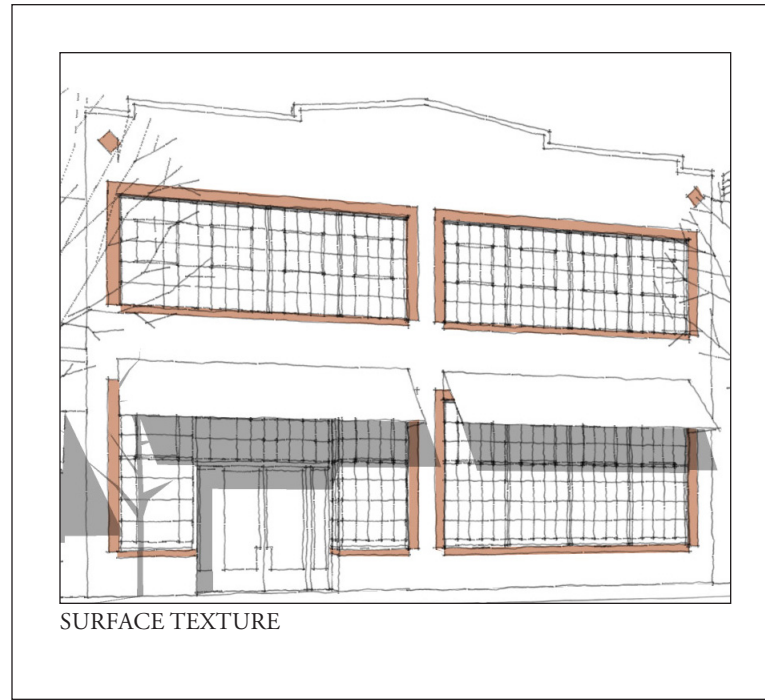


Village Mid-Century Storefronts continue the general themes of the style. Larger in width than height, they contain a majority of glazing punctuated by the entry doors to the interior. Metal window and door frames with thin profile sashwork are most common in order to maintain a light infill appearance. Wood is discouraged due to its heavier appearance.

Within the general rectangular openings the entry doors may be placed in any desired composition as symmetry at this smaller scale is less necessary for success. Entries should be larger than normal, often exceeding one door in width, and interesting industrial openings are encouraged. Other building bays may have additional entry points but the main entry should always be clear to the pedestrian.

The simplest way to accomplish this clarity is by placing the store signage directly over the main entry point, either on the awning or applied directly to the wall above the opening. Larger scale signage to be viewed from afar is best located on the parapet wall surface above the upper story windows as depicted.





Village Mid-Century walls are of masonry construction with a veneer of brick or less often stucco. Brick is encouraged, especially selections with sharp clean machine like edges and smooth color tone with minimal variations. Accent or contrasting color bricks, or alternate brick bond patterns are encouraged to emphasize the architectural elements and the horizontal plane. While they are encouraged they should also be used with restraint, as an exuberant use of this would not be fitting to the style.



Village Mid-Century awnings have sharp lines and visually 'hard' fabrics or materials. No soft curves or frilly, paisley flapping should occur in a Village Mid-Century awning. If sloped they have stiff metal frames with either taut, solid color fabrics or simple metal roofing. The pitch of the awning can range from nearly flat to nearly vertical. Flat awnings can range from running the full width of the building to only projecting out over the entry point. Flat awning thicknesses should never exceed 12" in height unless the larger size is proportional to the mass of the building and is needed for proper structure. Similar to the treatment of the windows, the structure (not the fabric or other covering) of a Village Mid-Century awning should be 'light' in appearance.



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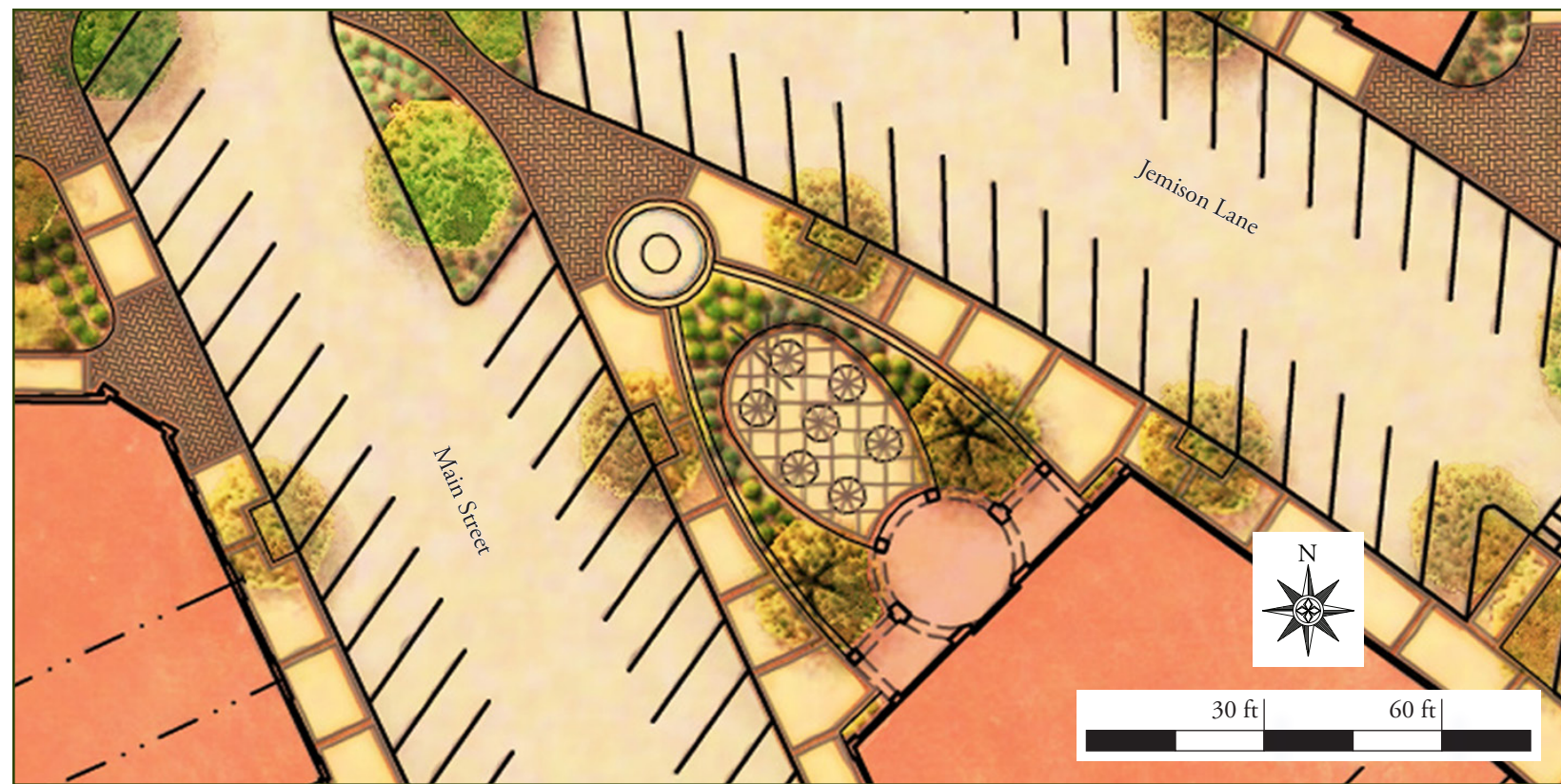


Lane Parke

LANDSCAPE STANDARDS

The standards set forth in this Section 10 shall apply to the design of the parks and open spaces within Lane Parke. The final designs of the parks, open spaces, streetscapes, sidewalks, curbs and paving shall be designed and constructed in accordance with the standards set forth in this Section 10 (the "Landscape Standards") and shall be subject to the Design Review Process. Example diagrams have been included in this Landscaping, Parks and Open Spaces Plan as reference material, provided however, such Design Examples shall not be construed as final designs. Final landscaping designs may vary from the Design Examples shown in this Landscaping, Parks and Open Spaces Plan provided such designs substantially conform to the Landscape Standards.





HARDSCAPES

Sidewalk, curb and paving designs, patterns and materials located on Primary Frontages and Secondary Frontages shall be compatible and consistent with the designs, patterns and materials of sidewalks, curbs and paving currently existing in the Village. The scale will be pedestrian friendly and encourage flow throughout the village. Enhanced design features may be permitted in accordance with the following:

All walkways located on Primary Frontages and Secondary Frontages will carry out the general theme of paving in the Village. Sidewalk corners at street intersections located on Primary Frontages and Secondary Frontages will have the same wood mold brick in a diagonal herringbone pattern with flat rowlock and soldier border as currently used. This will provide a seamless transition from the existing urban fabric to the new construction.

Beyond the corners, the sidewalks located on Primary Frontages and Secondary Frontages will be paved in concrete using the same scored running bond pattern, with the wood mold brick border. Stone accents will be used in a field of brick in special areas, such as drop off for the inn, plazas, pocket parks, some of the anchor stores and all corners at intersections located on Primary Frontages and Secondary Frontages.

Sidewalks on Primary Frontages shall be between eight (8) and twelve (12) feet wide, except that the sidewalks located on that portion of Lane Park Road north of Park Lane Court South shall be between six (6) and twelve (12) feet wide. Sidewalks on Secondary Frontages and Private Frontages shall be between six (6) and twelve (12) feet wide except that sidewalks in the Residential Use Area shall be a minimum of five (5) feet wide in accordance with the Residence G Standards. Notwithstanding the foregoing, the sidewalks surrounding the Inn/Commercial Use Area may be in accordance with the Inn/Commercial Use Area Sidewalk and Lighting Plan attached hereto as Appendix F. Streets will be paved in asphalt with concrete curb and gutter.

IDENTIFYING FEATURES

1. PEDESTRIAN FRIENDLY
2. A FIELD OF WOOD MOLD BRICK USED AT CORNERS ON PRIMARY FRONTAGES AND SECONDARY FRONTAGES.
3. SCORED BUFF CONCRETE WITH WOOD MOLD BRICK BANDS FOR ACCENT ALONG PRIMARY FRONTAGES AND SECONDARY FRONTAGES.
4. STONE ACCENTS AND STAINED CONCRETE USED AT SPECIAL ENTRANCES AND PLAZAS.
5. SMOOTH TRANSITION OF MATERIALS FROM EXISTING MOUNTAIN BROOK VILLAGE



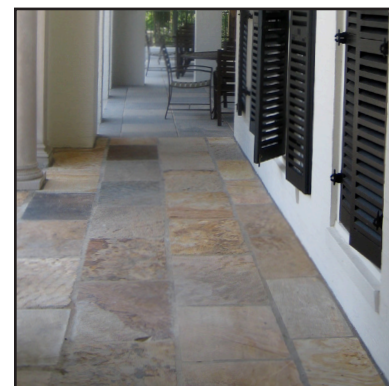
Brick Crosswalks



Scored Buff Concrete with Brick Bands



Brick at Sidewalk Corners



Special Stone Surfaces



STREET PLANTINGS

Trees and plantings shall be compatible and consistent with those used throughout the Village along Primary Frontages, Secondary Frontages, and along and within parking areas. Planters, benches and other similar features may be included, provided they are consistent with and complementary to those utilized throughout the Village. Enhanced design features may be permitted in accordance with the following:

A major characteristic of Mountain Brook is the lush tree canopy of the city. The majestic oaks at the intersection of Cahaba Road and Montevallo Road are iconic to Mountain Brook Village. The main reason these tall giants achieved such size in a streetscape situation is because of the large amount of root volume space in the islands. The large islands have allowed sufficient area for growth to reach maturity.

Street trees in small pits surrounded by paving do not achieve their true mature size or reach the potential longevity. It is our intent to provide a series of islands (adjacent to parking spaces) along Primary Frontages, Secondary Frontages, and along and within parking areas with ample space to allow enough root volume to achieve a similar size. These islands will be placed at each corner along the streetscape of Primary Frontages and Secondary Frontages to create an allee effect with a larger space size. These large shade tree plantings will eventually tower over any merchant signage per the described preference in the Village Master Plans.

Each street along Primary Frontages and Secondary Frontages will have a different species of large shade tree for the corners. Between the larger shade trees will be another species of a medium size shade tree. A low growing ground cover will be used predominately in these islands with a double row of evergreen shrubs. Unlike shrubs, the ground cover will not be as competitive with the tree for root growth and water but add another layer of green to the streetscape.

Most trees will be indigenous (or accepted cultivars) to the area and appropriate for the microclimate, suitable for urban conditions and unhindered pedestrian flow. Providing native trees will blend with the existing canopies in adjacent neighborhoods, village canopy and tree massing at the Botanical Gardens. There are a few accepted exotics trees, appropriate for streetscapes, which will be acceptable such as Elm varieties and Chinese Pistache.

SHRUBS, GROUND COVER AND VINES

Shrubs will occur where there are larger areas of walkway such as pocket parks, plazas, parks, sidewalks wider than ten feet, Cahaba Road or when screening is needed. Shrubs will not occur unless there is ample room for pedestrians to flow smoothly through an area. Vines will be planted at the base of columns in colonnade areas for additional greenery with evergreen flowering plants such as Carolina Jessamine and Confederate Jasmine.

IDENTIFYING FEATURES

1. ISLANDS TO SUPPORT LARGE SHADE TREES AT CORNERS ALONG PRIMARY FRONTAGES AND SECONDARY FRONTAGES
2. LARGE AND MEDIUM SIZE NATIVE SHADE TREES
3. EVERGREEN AND FLOWERING ACCENT TREES TO COMPLEMENT THE SHADE TREES





OPEN SPACE, STORM WATER MANAGEMENT AND ACTIVE CONNECTIVITY

Open space within Lane Parke shall include the Village Green, the Woodland Park, residential green space and numerous pocket parks and spaces of passage.

It is the goal to manage stormwater leaving the site and improve water quality. Maximizing water percolation into the ground is key to the management of the amount of runoff in a development. Lane Parke will strive to use a number of techniques like bioretention or raingardens, pervious paving in strategic locations and infiltration swales to assist in the percolation of water into the ground. All these methods filter the water and improve water quality.

All parks and public spaces are connected by a walkway system providing linkage to streetscapes and beyond to the Village and adjacent neighborhoods. These walkways will complement the existing network identified in the

Mountain Brook Pedestrian Master Plan and beyond providing linkage to the proposed greenways and trails of the Our One Mile Greenway Master Plan for Jefferson County.

IDENTIFYING FEATURES

1. VILLAGE GREEN
2. WOODLAND PARK
3. RESIDENTIAL GREEN SPACE
4. ACTIVE CONNECTIVITY WITH THE MOUNTAIN BROOK
5. INNOVATIVE STORM WATER MANAGEMENT
6. TREE LINED STREETS



VILLAGE GREEN

The Village Green will be located at the intersection of Jemison Lane and Lane Park Road and is the northern tip of this block. The green shall be designed and constructed in accordance with the following:

The Village Green will be a formal green space for the Village and provide a civic space for public gathering and passive use. This green space provides a continuous green canopy from the Birmingham Botanical Gardens in the west into Lane Parke and along Jemison Lane connecting with the natural corridor of Watkins Brook in the east. Native trees seen in both natural areas, in the eastern and western borders, will be used in the park and along the streetscape to strengthen that green connection. Doing so increases street tree sustainability and is in keeping with the natural context of the area with seasonal interest.

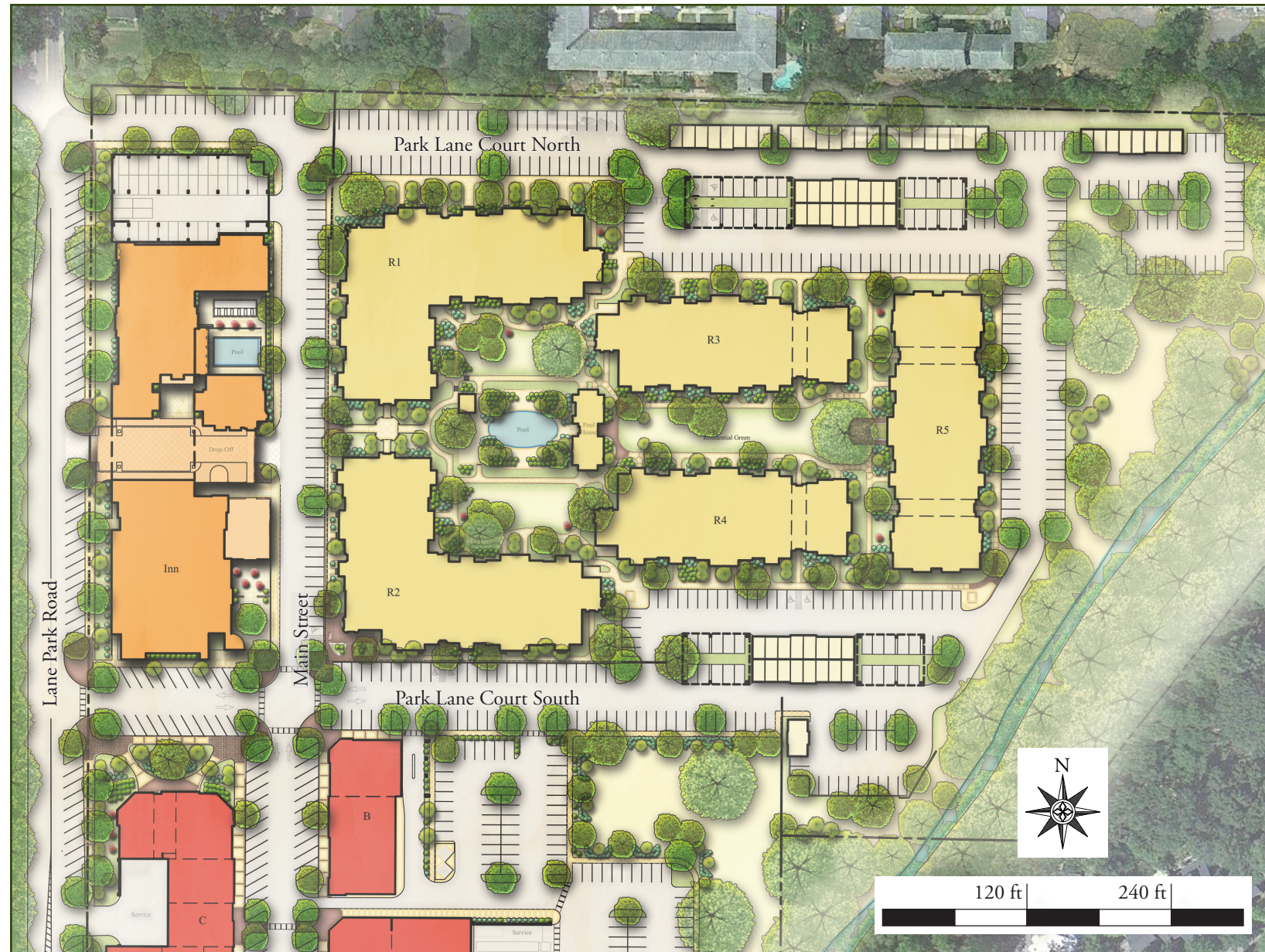
A central plaza will be the focal point of the park with an opportunity for artistic expression in the way of a fountain, sculpture or plantings. This plaza

area acts as an outdoor room for the adjacent buildings and transition to the lawn space and streetscapes beyond. Occasional seating is included with street lights and other site furnishings. A formal lawn space for passive play and gathering is surrounded by a perimeter walkway connecting the green to the network of traditional walkways found throughout the Village and beyond.

IDENTIFYING FEATURES

1. FORMAL LAWN FOR CIVIC EVENTS AND GATHERINGS
2. OCCASIONAL SEATING
3. BIORETENTION IN THE LAWN
4. PEDESTRIAN CONNECTIVITY
5. GREEN CONTINUITY WITH SURROUNDING AREAS
6. OUTDOOR DINING





RESIDENTIAL GREEN SPACE

Residential green space will be located around the inn, residences, creek and as buffer to parking areas in accordance with the Open Space Requirements. These areas are pedestrian friendly with connectivity to all retail spaces, parks and to the Village beyond. Selected existing shade trees of good health have been identified to be protected and incorporated in the new landscape. These mature specimens mixed with new plantings will create a quiet and mature look for the residences. A diverse selection of native shade trees, evergreen and flowering accent trees will be used throughout the area.

The canopy will blend in with the surrounding canopies of adjacent residential neighborhoods and the Birmingham Botanical Gardens across the street. Shrubs and groundcover will provide seasonal interest with a varied texture and color palette. Native plants will be incorporated to provide interest for local birds and wildlife.

The lawn will also be designed with bioretention infrastructure aiding storm water to percolate in the ground thus minimizing storm water runoff. Overflow will travel to Woodland Park for another opportunity of infiltration before reaching the creek.

IDENTIFYING FEATURES

1. QUIET AND PEDESTRIAN FRIENDLY
2. PEDESTRIAN CONNECTIVITY TO RETAIL SPACES, PARKS AND BEYOND
3. A DIVERSE SELECTION OF NATIVE SHADE TREES
4. EVERGREEN AND FLOWERING ACCENT TREE
5. VARIED PALETTE OF SHRUBS AND GROUND COVER FOR SEASONAL INTEREST
6. SELECTED HEALTHY EXISTING TREES PROTECTED



WOODLAND PARK

The Woodland Park will be located as shown on the Illustrative Master Plan and shall be completed by the completion of construction of the Residential Phase.

Plantings within the natural area will include riparian plants for creek stabilization and indigenous trees planted beyond the creek (similar to the eastern slope to bring that canopy type throughout the Woodland Park and reminiscent to the canopy at the Botanical Gardens to the west of Lane Park Road). Native plant massing will be used throughout and with areas of native shrubs, groundcover and wildflowers.

Walking paths, similar to the Jemison Trail, will connect the Village with the natural areas and designated areas for wildlife observation, picnics and informal gathering and walks. These paths via the walkway system will connect with the existing Mountain Brook trail system. The exposed creek will be reminiscent to a rock lined mountain stream with natural plantings.

Plants will be watered initially for stabilization only. Long term irrigation will not be used. Bioretention will be used to handle storm water overflow and filter toxins present before entering Watkins Branch.

IDENTIFYING FEATURES

1. NATIVE TREE CANOPY
2. TRAILS
3. CONNECTIVITY WITH MOUNTAIN BROOK TRAIL SYSTEM
4. NATIVE PLANTS AND WILDFLOWERS
5. WILDLIFE OBSERVATION
6. COMPLEMENTS ADJACENT EXISTING TREE CANOPIES
7. OCCASIONAL SEATING



PLANT LIST

GREEN SPACE

SHADE TREES

Latin Name	Common Name
Fagus grandifolia	American Beech
Fraxinus pennsylvanica	Green Ash
Fraxinus pennsylvanica 'Patmore'	Pat Moore Ash
Fraxinus pennsylvanica 'Urbanite'	Urbanite Ash
Ginkgo biloba	Ginkgo
Koelreuteria bipinnata	Golden Rain Tree
Liquidambar styraciflua	Fruitless Sweetgum
Liriodendron tulipifera	Tulip Poplar
Magnolia grandiflora	Southern Magnolia
Magnolia macrophylla	Big Leaf Magnolia
Nyssa sylvatica	Black Gum
Pistacia chinensis	Chinese Pistache
Quercus alba	White Oak
Quercus falcata	Southern Red Oak
Quercus lyrata	Overcup Oak
Quercus michauxii	Swamp Chestnut Oak
Quercus nuttallii	Nuttall Oak
Quercus phellos	Willow Oak
Quercus shumardii	Shumard Oak
Taxodium distichum	Cypress
Ulmus parvifolia	Lacebark Elm
Ulmus parvifolia "Bosque"	Bosque Elm
Ulmus parvifolia "Drake"	Drake Elm

SMALL ACCENT TREES

Latin Name	Common Name
Acer palmatum	Japanese Maple
Betula nigra	River Birch
Cercis canadensis	Redbud
Cornis florida	Dogwood
Crataegus marshallii	Parsley Hawthorn
Ilex cornuta "burfordii"	Burford Holly
Ilex decidua	Deciduous Holly
Ilex vomitoria	Yaupon Holly
Lagerstroemia indica	Crape Myrtle
Magnolia virginiana 'Australis'	Sweet Bay Magnolia
Malus x zumi	Zumi Crabapple
Myrica cerifera	Wax Myrtle

VINES

Latin Name	Common Name
Ficus pumila	Fig Vine
Gelsemium sempervirens	Carolina Jessamine
Parthenocissus tricuspidata	Boston Ivy
Smilax lanceolata	Smilax
Trachelospermum asiaticum	Asiatic Jasmine
Trachelospermum jasminoides	Star Jasmine

NATIVE PLANTS FOR NATURAL AREAS

Latin Name	Common Name
Aesculus pavia	Red Buckeye
Ageratum	Hardy Ageratum
Asimina triloba	PawPaw
Asclepias tuberosa	Butterfly Weed
Betula nigra	River Birch
Callicarpa americana	American Beautyberry
Calycanthus floridus	Carolina Sweetshrub
Carpinus caroliniana	American Hornbeam
Cercis canadensis	Redbud
Chionanthus virginicus	Fringe Tree
Clethera accuminata	Summersweet
Coreopsis	Coreopsis
Cornus florida	Flowering Dogwood
Crataegus marshallii	Parsley Hawthorn
Daucus corata	Queen Annes Lace
Euonymus americana	Hearts a Bustin'
Fagus grandifolia	American Beech
Fothergilla gardenia 'Mt. Airy'	Fothergilla selections
Halesia diptera	Silver Bell
Hibiscus militaris	Swamp Hibiscus
Hydrangea quercifolia	Oakleaf Hydrangea
Ilex decidua	Deciduous Holly
Kalmia latifolia	Mountain Laurel
Magnolia macrophylla	Bigleaf Magnolia
Magnolia virginiana	Sweet Bay Magnolia
Prunus americana	Wild Plum
Rudbeckia	Black Eye Susan
Schizachyrium scoparium	Little Bluestem
Violet odorata	Sweet Violet

SHRUBS AND GROUND COVER

Latin Name	Common Name
Abelia grandiflora	Glossy Abelia
Agarista populifolia	Florida Leucothoe
Aspidistra elatior	Aspidistra
Berberis thunbergii	Red Leaf Barberry
Clethera accuminata	Summersweet
Cotoneaster horizontalis	Cotoneaster
Cotoneaster lacteus	Parney's Cotoneaster
Cyrtomium falcatum	Holly Leaf Fern
Euonymus alatus	Winged Euonymus
Euonymus americana	Hearts a Bustin'
Forsythia intermedia	Forsythia
Fothergilla gardenia 'Mt. Airy'	Fothergilla selections
H. quercifolia 'Alice'	Oakleaf Hydrangea
H. quercifolia 'Pee Wee'	Oakleaf Hydrangea
H. quercifolia 'Snow Flake'	Oakleaf Hydrangea
H. quercifolia 'Snowqueen'	Oakleaf Hydrangea
Hamamelis virginiana	Witchhazel
Ilex cornuta "Burfordii"	Burford Chinese Holly
Ilex latifolia	Luster-Leaf Holly
Ilex vomitoria	Yaupon
Ilex x messerve 'Blue Princess'	Blue Princess Holly
Ilex x 'Nellie Stevens'	Nellie Stevens Holly
Jasminum floridum	Florida Jasmine
Jasminum mesnyi	Primrose Jasmine
Juniperus conferta	Shore Juniper
K. latifolia 'Minuet'	Mountain Laurel (Dwarf)
K. latifolia 'Sarah', 'Nathan Hale'	Mountain Laurel
Leucothoe fontanesiana	Leucothoe
Liriope muscari	Liriope
Myrica cerifera	Wax Myrtle
Nandina domestica	Nandina
Ophiopogon japonicus	Mondo
Prunus laurocerasus 'Schipkaensis'	Skip Laurel
Raphiolepis indica	Indian Hawthorn
Rosa 'Radrazz'	Knockout Rose
Spiraea cantoniensis	Reeves Spirea
Spiraea thunbergii	Baby's Breath Spirea
Spiraea vanhouttei	Vanhoutte Spirea
Ternstroemia gymnanthera	Cleyera
Vinca major	Vinca

RIPARIAN PLANTS FOR CREEK STABILIZATION

Latin Name	Common Name
<i>Trees</i>	
Acer rubrum (FAC)	Red Maple
Betula nigra (FACW)	River Birch
Cornus amomum	Silky Dogwood
Cornus foemina (FACW-)	Swamp Dogwood
Magnolia virginiana (FACW+)	Sweet Bay Magnolia
Nyssa aquatica (OBL)	Water Tupelo
Quercus michauxii	Swamp Chestnut Oak
Quercus nigra (FAC)	Water Oak
Quercus phellos (FACW-)	Willow Oak
Quercus shumardii (FACW-)	Shumard Oak
Salix nigra (OBL)	Black Willow
Taxodium distichum (OBL)	Bald Cypress
<i>Grasses/Herbs</i>	
Amorpha fruticosa (FACW)	False Indigo
Andropogon gerardii (FAC)	Big Blue Stem
Asclepias incarnata (OBL)	Milkweed
Aster sp.	Aster
Echinacea purpureum	Purple Cone Flower
Eupatorium purpureum	Joe-Pye Weed
Helenium autumnale (FACW)	Sneeze Weed
Helianthus simulans	Swamp Sunflower
Juncus effusus	Common Rush
Lobelia cardinali	Cardinal Flower
Panicum virgatum	Switch Grass
Schizachyrium scoparium (FACU)	Little Blue Stem
Scirpus sp.	Wool Rush
Sorghastrum nutans (FACU)	Indian Grass
Tripsicum dachtyoides	Gamma Grass
<i>Shrubs</i>	
Cephalanthus occidentalis (OBL)	Buttonbush
Hydrangea querciflora (FACU)	Oak leafed Hydrangea
Ilex cassine (FACW)	Daboon Holly
Itea virginica (FACW+)	Virginia Sweetspire
Myrica cerifera (FAC+)	Wax Myrtle
Rhododendron sp.	Native Azalea
Viburnum nudum (FACW+)	Poosumhaw
<i>Ferns</i>	
Dryopteris erythrosora	Autumn Fern
Onoclea sensibilis (FACW)	Sensitive Fern
Osmunda sp. (FACW+)	Cinnamon Fern
Woodwardia areolata (OBL)	Netted Chain Fern

The standards set forth in this Section 11 shall apply to the design of lighting and screening within Lane Parke (the "Lighting and Screening Standards").

LIGHTING

Lighting fixtures will be provided throughout Lane Parke sufficient to produce an average of 1.5 foot candles of light for all public areas for the comfort and safety of patrons and residents. The light fixtures will be thoughtfully placed so as not to detract from the architecture but enhance the massing and details of the varied styles, and generally in accordance with the following:

STREET LIGHTING

The traditional acorn styled street light will be used on all streets within Lane Parke. Light poles will be appropriately spaced to provide an average of 1.5 foot candles of light coverage. In addition, ground up lighting will be placed at all street tree locations and where appropriate at building facades. Storefronts will contribute to the light levels due to the large amount of glass area required for each building facade. Notwithstanding the foregoing, the street lighting for the Inn/Commercial Use Area may be in accordance with the Inn/Commercial Use Area Sidewalk and Lighting Plan attached hereto as Appendix F.

BUILDING FACADES

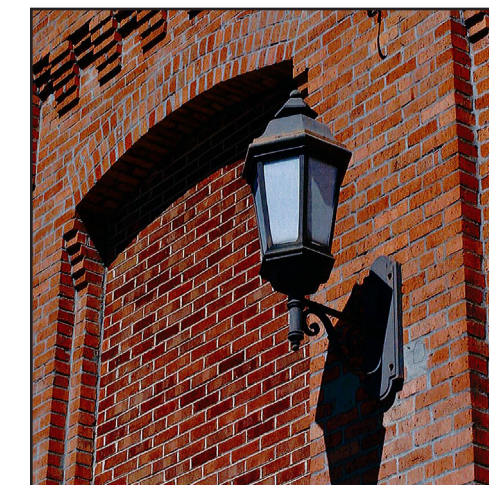
A variety of wall sconces and recessed lighting fixtures will be used throughout Lane Parke, which will be located at appropriate locations in order to highlight entrances, public walkways and specific shops. Architectural accent lighting will also be used and will be located on the rear of parapets and on the roofs to highlight special architectural features.

STRUCTURED PARKING

The primary source of illumination for any Parking Structure should be light fixtures hung from the ceiling and appropriately spaced. On the roof top level of any Parking Structure, lighting shall be supplied by recessed wall mounted or similar fixtures in order to limit the amount of light spillage. No pole lights shall be employed on the roof top level of any Parking Structure.

IDENTIFYING FEATURES

1. CLASSIC ACORN STREET LIGHT
2. SUBTLE ILLUMINATION ON STREET – NOT TOO BRIGHT
3. ACCENT LIGHTING ON ARCHITECTURAL FEATURES AND TREES
4. BRIGHT ILLUMINATION IN PARKING DECKS



SCREENING

All mechanical equipment will be screened so that no portion of the equipment is visible from an eye level view or from below. Screening materials shall be complementary to the building materials used in adjacent structures. Where reasonably possible, forms of trellises will be used as a roof element to provide supplemental screening for such equipment. To the extent mechanical equipment is visible from above, reasonable measures shall be employed to minimize the visibility of such equipment, including painting such equipment to match the screening enclosure or the roof.



The standards set forth in this Section 11 shall apply to the design of signs and awnings within Lane Parke (the "Signage Plan"), and all signs and awnings shall be approved by the Design Committee in accordance with the requirements of the City Code.

VILLAGE ROMANTIC

The most common types of signage in this style are painted plaques hung from a single decorative bracket mounted perpendicular to the face of the building. Other types of signage include those printed on awnings over entrances, stenciled/painted script on linear plaques over entrances and stenciling on storefront glass.

Typical sign size classification: 2' x 3' for hanging signs – Other types will vary with building configuration.



SOUTHERN MERCANTILE

Large and bold lettering mounted (or stenciled) onto friezes and/or head casing of storefronts, as well as painted lettering on awnings over entrances and storefront glass are the most common types of signage in this style. Painted masonry (frequently on the sides of corner buildings) and hanging signs similar to those found in Village Romantic are also common.

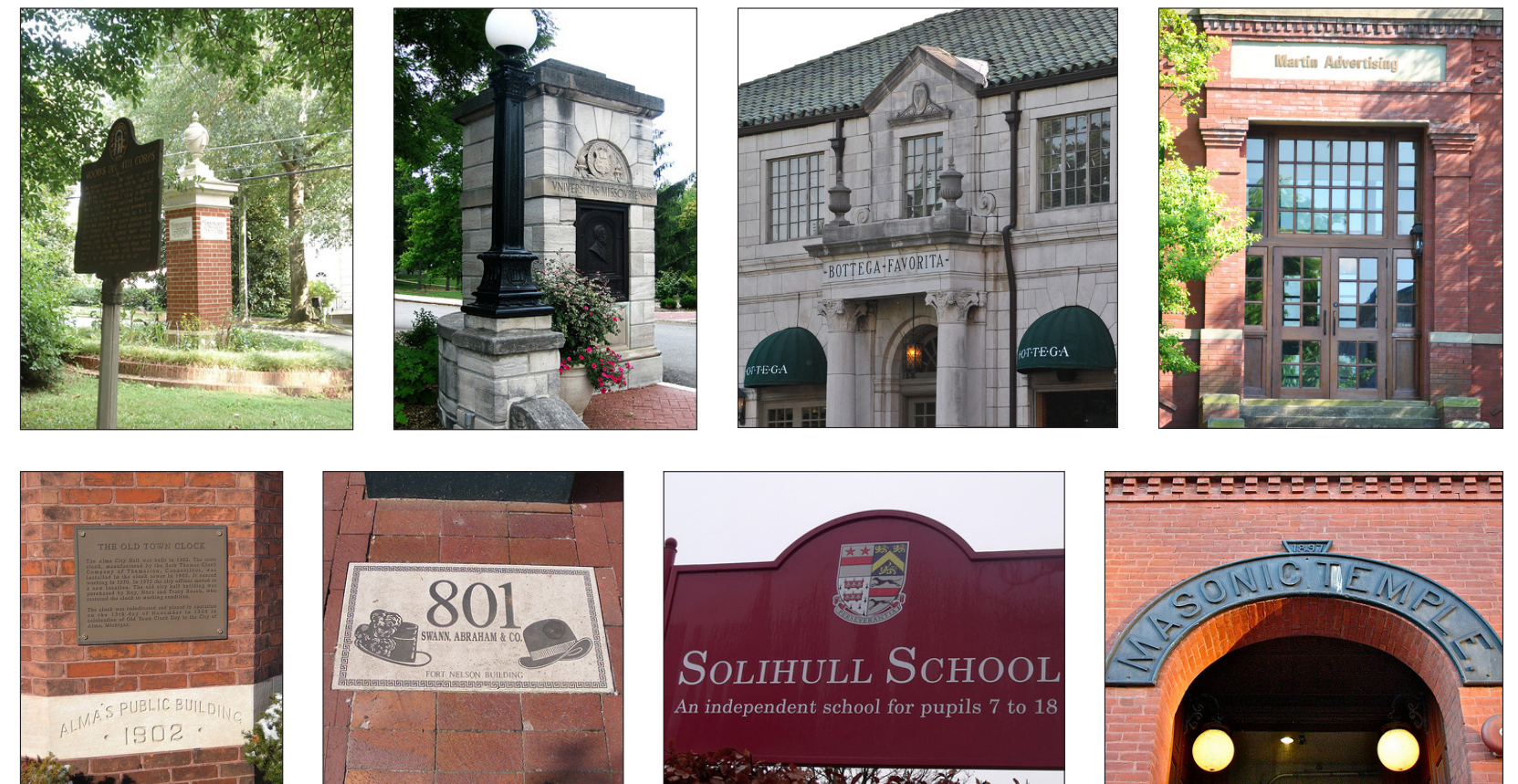
Typical signage size classification: 6-8' x 2' for storefront signage. Painted masonry can cover large expanses of side walls. Other types will vary with building configuration.



BIRMINGHAM CLASSIC

Signage tends to be less bold than in other styles. The highest style examples will have the building or occupant's name carved into the frieze or pediment. Small plaques either wood or cast metal are also frequently mounted on the building surfaces as well as lettering on awnings. Site permitting, classically detailed free standing signs or monuments are also appropriate.

Typical signage size classification: 3-4' x 6-12" sized to fit within frieze applications. Wall mounted plaques and free standing signage will vary with building configuration.



VILLAGE MID-CENTURY

The more austere surfaces of these buildings are often enriched by large painted or mounted signs over entrances and/or parapets. Awnings with stenciled or mounted signage or perpendicular hanging signs also can add liveliness to this style.

Typical signage size classification: Painted signage can span across several bays of a building's façade. Mounted signage tends to be of a similar scale to those found on Southern Mercantile buildings, however it can and often is applied to a building in a vertical orientation.



PARKING

To service the parking demands of the improvements to be constructed within Lane Parke, not less than 1,099 parking spaces are projected to be provided (the “Total Parking Supply”).

Parking spaces in excess of the Total Parking Supply may be provided.

Based upon the methodology set forth herein, the Total Parking Supply will be sufficient to (i) satisfy existing zoning standards and (ii) meet the anticipated parking demand created by the uses and improvements of Lane Parke.

The Total Parking Supply satisfies the requirements of existing zoning when using the shared parking methodology permitted by the Village Overlay Standards. The Village Overlay Standards require that approximately 1,231 parking spaces be provided (the “Code Parking”) to service the uses and improvements to be included within Lane Parke. The Code Parking reflects the number of parking spaces required for non-shared uses under Section 129-555(a) of the Village Overlay Standards, as reduced for shared parking arrangements in accordance with Section 129-555(b)(3)(d) thereof, which provides that “the cumulative requirements of uses sharing the parking may be reduced for uses having different hours of operation or different peak periods of demand . . . [provided the] proposed reduction shall be based upon recognized industry sources, such as the most recent edition of the Urban Land Institute Shared Parking Model.” As more particularly set forth in the Shared Parking Analysis (as defined below), the Code Parking was determined by applying the shared parking model established by the Urban Land Institute.

To evaluate anticipated parking demand within Lane Parke, Walker Parking Consultants (“Walker”), a leading national parking consultant, has prepared a Shared Parking Analysis, a copy of which has been submitted with this PUD Application as Appendix A (the “Shared Parking Analysis”). In preparing the Shared Parking Analysis, Walker has utilized the methods prescribed by *Shared Parking, 2nd Edition* (as updated as of January, 2005), an industry standard publication commissioned and approved by the Urban Land Institute and the International Council of Shopping Centers.

The Shared Parking Analysis concludes, among other things, as follows:

- 223 parking spaces are required to serve Lane Parke during periods of peak demand (7:00 p.m. on a Saturday in December).
- Surplus parking is projected to be available 365 days per year.

In accordance with the recommendations of the Shared Parking Analysis, the Total Parking Supply shall be provided, subject only to Permitted Attrition (as defined below), in order to meet projected demand and satisfy the Code Parking.

“Shared Parking Opinion” shall mean an opinion, addressed to the City, issued by Walker or another nationally recognized parking consultant approved by the City certifying that any increase in the square footage of any specific use in excess of the applicable Shared Parking Assumption for such specific use shall not create parking demand in excess of the Total Parking Supply. If the opening for business by an occupant of space in Lane Parke shall cause the square footage of any specific use (as described in the table below) to exceed the Shared Parking Assumption for such specific use, the Project Owner shall deliver to the zoning officer, prior to such occupant’s opening for business, a Shared Parking Opinion. The Project Owner shall have the right to increase the Total Parking Supply through the use of valet services, off-site parking arrangements, the construction of additional parking spaces or otherwise in order for a Shared Parking Opinion to be issued.

“Shared Parking Assumptions” shall mean:

Land Use	Factor	Unit
Retail	47,415	square feet of Net Leasable Area
Fine/Casual Dining	24,500	square feet of Net Leasable Area
Fast Food	3,400	square feet of Net Leasable Area
Hotel	100	rooms
Hotel - Meeting Space	6,655	square feet of Net Leasable Area
Residential Shared, Rental	276	units
Bank	12,340	square feet of Net Leasable Area
Grocery	28,300	square feet of Net Leasable Area

“Permitted Attrition” shall mean the permitted reduction in the Total Parking Supply due to handicap parking requirements and permissible variations in the Lane Parke Plan, which shall not exceed 3% of the Total Parking Supply.



TRAFFIC AND ACCESS

The plan of traffic and access for Lane Parke shall be as more particularly set forth herein in this Section 13 (the “Traffic and Access Plan”). The traffic and access improvements constructed in connection with the Lane Parke Plan shall be in accordance with the Traffic and Access Plan or the Alternate Drive Through and Access Plan.

The Traffic and Access Plan has been prepared in accordance with the recommendations of a Traffic Impact Study prepared by Skipper Consulting, Inc, a leading regional traffic consultant, a copy of which has been submitted with this PUD Application as Appendix B (the “Traffic Impact Study”).

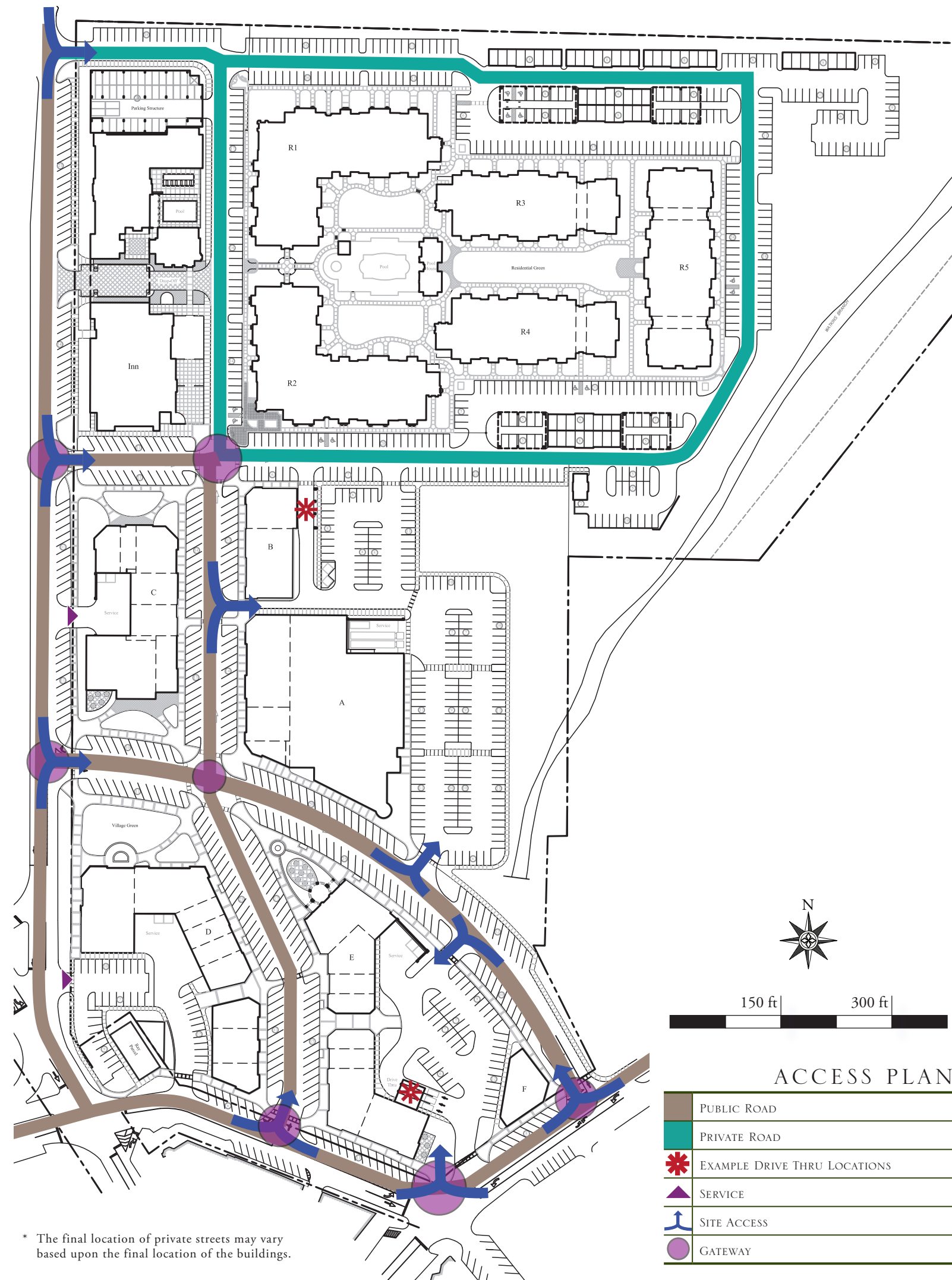
The Traffic Impact Study recommends that the following improvements be constructed to better service the traffic demands resulting from the uses and improvements to be included within Lane Parke (collectively, the “Traffic Improvements”):

- Widen Lane Park Road to a three lane cross section from the intersection of Cahaba Road/Culver Road/U.S. Hwy. 280 ramps for approximately 440 feet northward (including turn bay storage, turn bay taper, and transition taper).
- Widening of Culver Road to a three lane cross section.
- Restriping Montevallo Road to provide a left turn lane into the site access.
- Modifying the traffic signal at the intersection of Cahaba Road/Lane Park Road/Culver Road/U.S. 280 Ramps to provide a protected-permissive left turn arrow for traffic turning left from the U.S. 280 ramp northbound onto Cahaba Road and Lane Park Road.

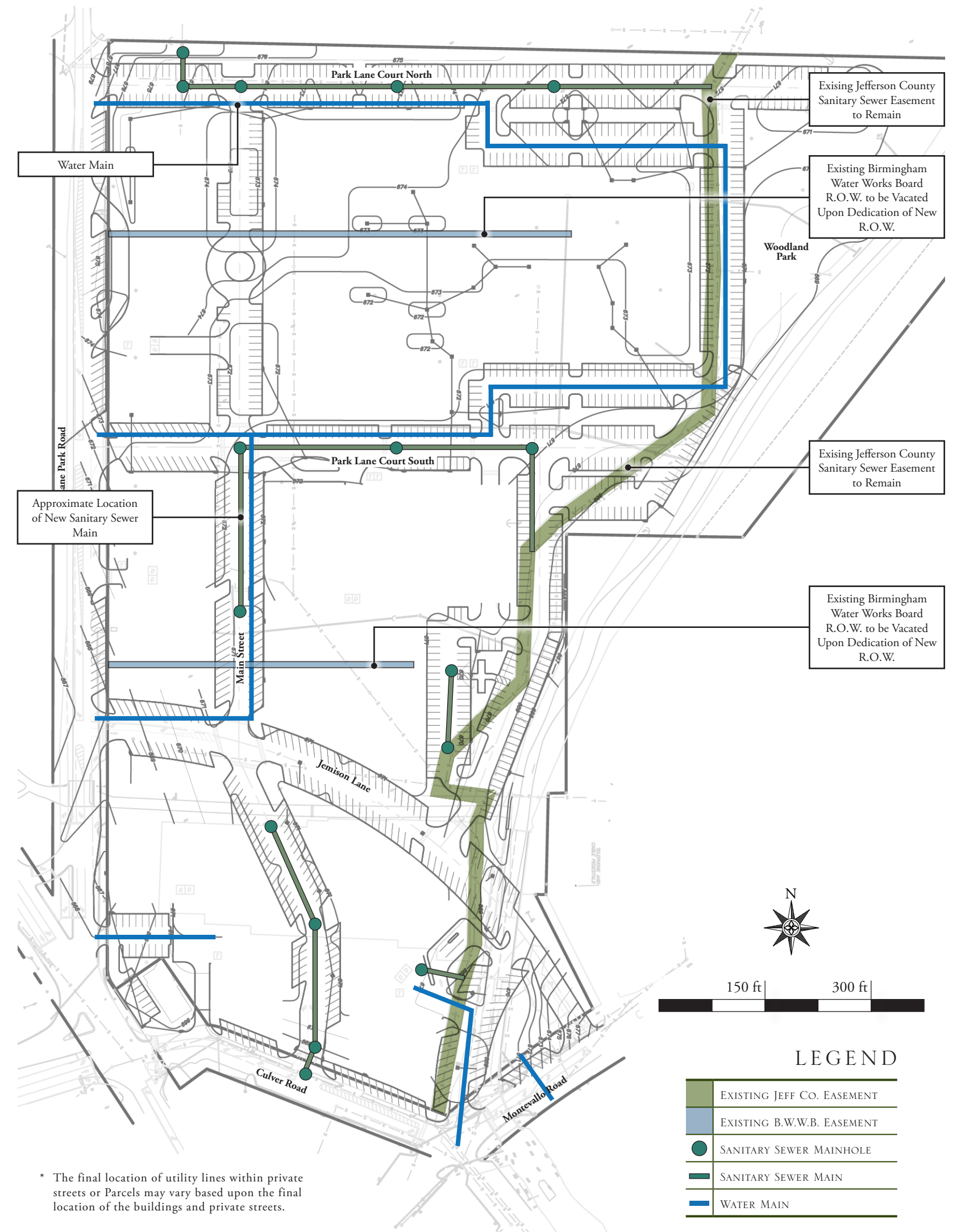
In accordance with the recommendations of the Traffic Impact Study, the Traffic Improvements shall be constructed and/or implemented.



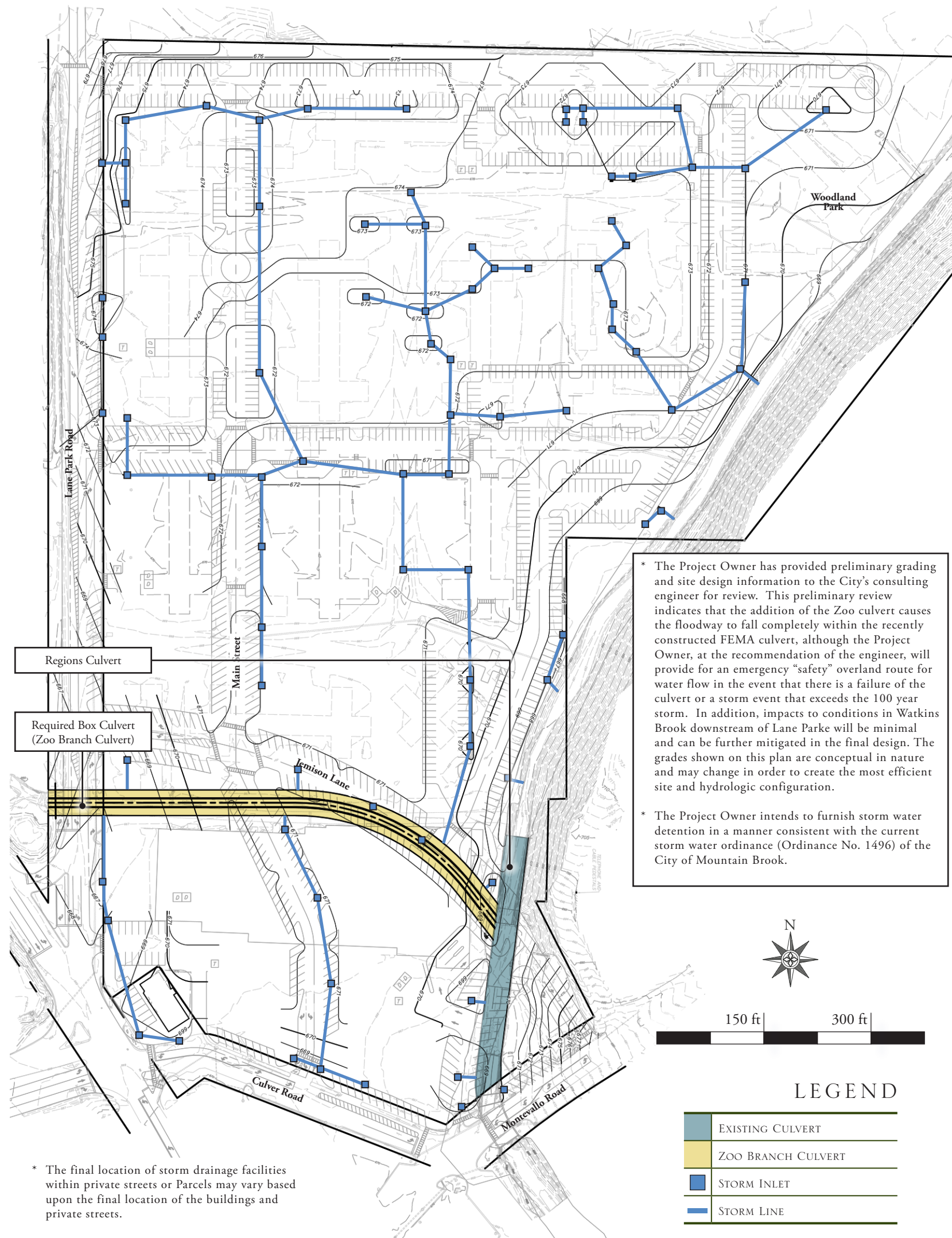
* - UPDATED FOR CONSISTENCY WITH ILLUSTRATIVE PLAN



UTILITIES PLAN



STORM DRAINAGE PLAN



Lane Parke is projected to be developed in four (4) Phases, as more particularly depicted below. However, at the election of the Project Owner, each of the Phases may be constructed concurrently, and nothing in this PUD Application shall be construed to require that the Phases be constructed in any particular order. The schedule of construction, subject to force majeure, is anticipated to be as follows:

RESIDENTIAL PHASE

Commencement will occur on or before the date that is one (1) year following approval of this PUD Application.

Substantial completion of improvements within the Residential Phase is anticipated to occur within twenty-six (26) months of commencement of construction of the Residential Phase.

GROCERY/COMMERCIAL PHASE

Commencement anticipated on or before the date that is twenty-six (26) months following approval of this PUD Application.

Substantial completion of improvements within the Grocery/Commercial Phase is anticipated to occur within twenty four (24) months of commencement of construction of the Grocery/Commercial Phase.

RESIDENTIAL PHASE AND GROCERY/COMMERCIAL PHASE



RETAIL PHASE

Commencement anticipated to occur following completion of improvements in the Grocery/Commercial Phase and subject to the provisions of current or future lease encumbrances which might affect the timing of work.

Substantial completion of improvements within the Retail Phase is anticipated to occur within twenty four (24) months of commencement of construction of the Retail Phase.

INN PHASE

Commencement anticipated on or before the date that is four (4) years following approval of this PUD Application.

Substantial completion of all improvements within the Inn Phase is anticipated to occur within twenty four (24) months of commencement of construction of the Inn Phase.

INN PHASE AND RETAIL PHASE



INTERIM CONSTRUCTION PHASE

An interim phase of development may be necessary in order to allow construction of certain portions of the development to continue while honoring lease obligations to existing tenants of the shopping center located on the Property (the "Shopping Center"). If necessary, this interim phase will include:

- Demolition of a portion of the Shopping Center comprised of the space which is currently leased for use as a grocery store and those portions of any adjacent space up to the load bearing eastern wall of the space currently leased for use as a pharmacy (the "Pharmacy Lease").
- Minor architectural improvements to the remaining portions of the Shopping Center (the "Remaining Structure"), which improvements shall be subject to advisory review by the Design Committee;
- Continued operations in the Remaining Structure by a limited number of tenants until the earlier to occur of (i) December 31, 2019 or the (ii) date on which the Pharmacy Lease is terminated.
- Construction of those portions of the Zoo Branch Culvert that can reasonably be constructed while the Remaining Structure is in place.
- Construction of a majority of Jemison Lane as shown on the Interim Construction Phase Improvement Plan, provided however, that the area of the Interim Construction Phase Improvement plan designated as "To Be Engineered" may be modified as necessary to temporarily connect Jemison Lane with Montevallo Road and/or Culver Road.

- Temporary connection of Jemison Lane to Montevallo Road and/or Culver Road by means of a two-lane road or drive improved with curbs and to be utilized primarily for through access to Jemison Lane;
- Development of temporary green space within the area in which the demolished portions of the Shopping Center were located that are not being used for access or parking.

The Interim Construction Phase may remain in place until the earlier to occur of (the "Pharmacy Lease Termination Date"): (i) December 31, 2019 or the (ii) date on which the Pharmacy Lease is terminated. At any time, following the Pharmacy Lease Termination Date, the City shall have the right to make written request of the Project Owner to demolish the Remaining Structure and complete the construction of Jemison Lane in its entirety (collectively, the "Road and Demolition Work"). If Project Owner has received written notice from the City to perform the Road and Demolition Work (the "City Notice"), the Project Owner shall commence the Road and Demolition Work within one hundred eighty (180) days of the City Notice and shall diligently pursue the completion of the Road and Demolition Work. In the event the Project Owner does not timely commence the Road and Demolition Work following receipt of the City Notice or does not thereafter diligently pursue the Road and Demolition Work to completion, the City shall have the right to enter the premises and to perform the Road and Demolition Work without further notice to Project Owner, with all costs of such Road and Demolition Work to be the responsibility of the Project Owner.

INTERIM CONSTRUCTION PHASE



INTERIM CONSTRUCTION PHASE



SUBDIVISION PLAT

As part of the Lane Parke Plan, the Property will be resurveyed and subdivided. On or before the commencement of construction of the first Phase in Lane Parke, the Project Owner will submit an application with the Planning Commission for approval of a preliminary subdivision plat creating Parcels and rights of way consistent with the approved Lane Parke Plan.

VACATION OF PUBLIC RIGHTS OF WAY

In order for the development of Lane Parke to commence, it will be necessary that certain public streets and rights of way located within Lane Parke be vacated. Such streets and rights of way are more particularly shown on that certain Map of Dedication recorded in Map Book 120, Page 34 in the Office of the Judge of Probate of Jefferson County, Alabama, a copy of which is attached as Schedule I to this Plan of Subdivision, Ownership, Maintenance and Restrictive Covenants. Prior to the commencement of construction of each Phase, the Project Owner will submit a Declaration of Vacation to be approved by the Planning Commission and City Council with respect to the applicable streets located in such Phase.

OWNERSHIP AND MAINTENANCE

The Project Owner is the owner of the Evson Parcel. RC Limited, LLC is the owner of the Ray Parcel.

The Project Owner intends to market the space in Lane Parke for lease to commercial or residential tenants, as applicable, though Parcels or Units may be sold as market conditions dictate. Improvements such as private streets, sidewalks, utilities, parking structures and parks will be conveyed to and owned by the Master Association and subjected to use restrictions pursuant to the Master Declaration (as defined below) and/or other recorded restrictions. Public streets and rights of way shall be conveyed or dedicated to the City. Improvements within the Open Space Use Area or the Public Use Area, regardless of how owned, will generally be open to the public without restriction or charge, except that limited areas of any Parking Structure, rights of way and sidewalks may be reserved for the exclusive use of occupants of certain Parcels or Units.

As more particularly described below, the Property will be subject to a Master Declaration that will provide reciprocal rights to and impose common obligations on each subdivided Parcel within Lane Parke. Additional restrictions and governance structures may be imposed in the form of condominium regimes or restrictive covenants, as described below.

The Master Declaration, the condominium regimes and the restrictive covenant regimes described below shall establish the framework through which the common areas of Lane Parke shall be maintained.

MASTER DECLARATION

The Property will be subject to a Master Declaration of Restrictions and Easements (the "Master Declaration"). The Master Declaration will grant reciprocal easements that benefit and burden all Parcels within the Property, impose restrictions on the use and development of the Parcels and establish protocols for the maintenance of common areas. Anticipated provisions to

be included in the Master Declaration are, but shall not be limited to, the following:

EASEMENTS

- Easements for ingress and egress over non-public rights of way, sidewalks, parking areas, open space and other common areas.
- Easements for parking over any non-public parking areas that are not otherwise specifically reserved for use by specific Parcels or Units within the Property.
- Easements for the right to enter upon and maintain common areas, including the right to cut and trim trees and other vegetation.
- Easements for the installation and maintenance of utilities.
- Easements for storm water drainage and for the installation and maintenance of the Property's storm water drainage system.
- Easements for lateral support for improvements constructed on or near common boundary lines.

RESTRICTIONS

- Restrictions on categories of use within certain Parcels or Units within the Property. By way of example, no uses other than residential uses will be permitted within Parcel 2.
- Prohibitions on specific uses with respect to certain Parcels and Units and with respect to the Property as a whole. By way of example, pawn shops would be a prohibited use in areas where retail is a permitted use.
- Restrictions and regulations governing the design and orientation of improvements on the Property, to the extent not addressed by the Lane Parke Plan.
- Rules and regulations governing the use and operation of the Property.

COMMON AREA MAINTENANCE

- A framework pursuant to which the Project Owner, as declarant, or, alternatively, a Master Association, shall be responsible for the maintenance of all non-public rights of way, sidewalks, parking areas, landscaping, open space and other common areas. The Project Owner or the Master Association, as applicable, shall have the right to levy assessments against each Parcel for such Parcel's pro-rata share of maintenance expenses, and the Project Owner or such Association shall have a continuing lien against such Parcel for non-payment of assessments. The assessment lien shall be subject to the lien of ad valorem taxes.



CONDOMINIUM REGIME FOR LANE PARKE

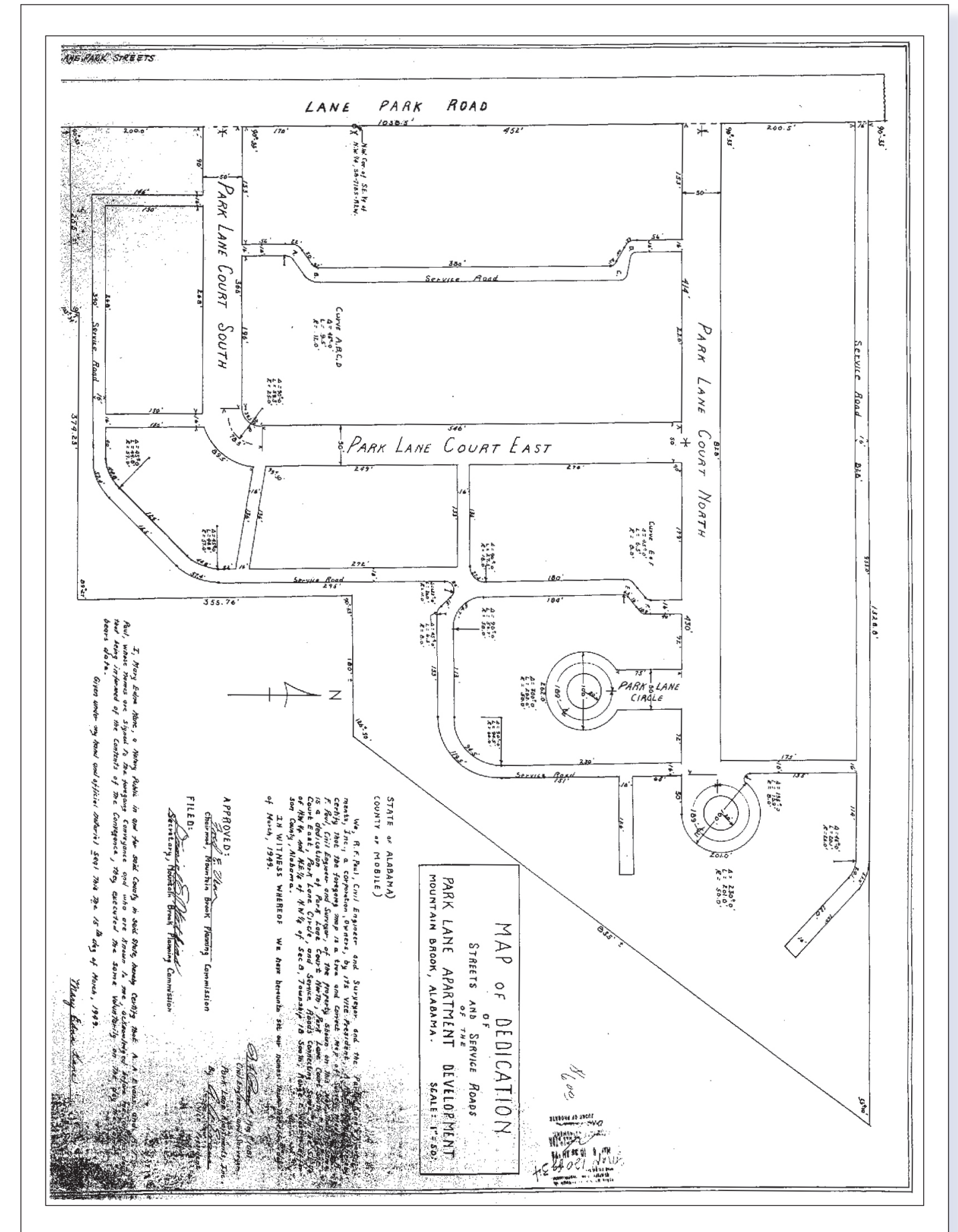
The Parcels in Lane Parke may be submitted to the condominium form of ownership pursuant to the provisions of the Alabama Uniform Condominium Act of 1991, Code of Alabama 1975, §§ 35-8A-101 *et seq.* through one or more condominium regimes (each, a "Condominium Regime"). The Units in any Condominium Regime shall consist of separate blocks of space or buildings within Lane Parke. To illustrate, the following are examples of potential Units within the condominium:

- the building footprints may be designated as land condominium Units;
- the space designated for use as an inn may comprise a Unit;
- blocks of space designated for use as retail or commercial may comprise one or more Units;
- the space designated for use as residential, or individual residential units, may comprise one or more Units;
- any Parking Structure may comprise a Unit; and
- the sidewalks and certain other common areas may comprise one or more Units.

Each Unit will be subject to a Declaration of Condominium (a "Condominium Declaration"). Any Condominium Declaration will establish the condominium Units, grant reciprocal easements that benefit and burden all Units within such Condominium Regime, impose restrictions on the use of the Units and establish protocols for the maintenance of common areas through a condominium association.



SCHEDULE I TO PLAN OF SUBDIVISION, OWNERSHIP, MAINTENANCE AND RESTRICTIVE COVENANTS

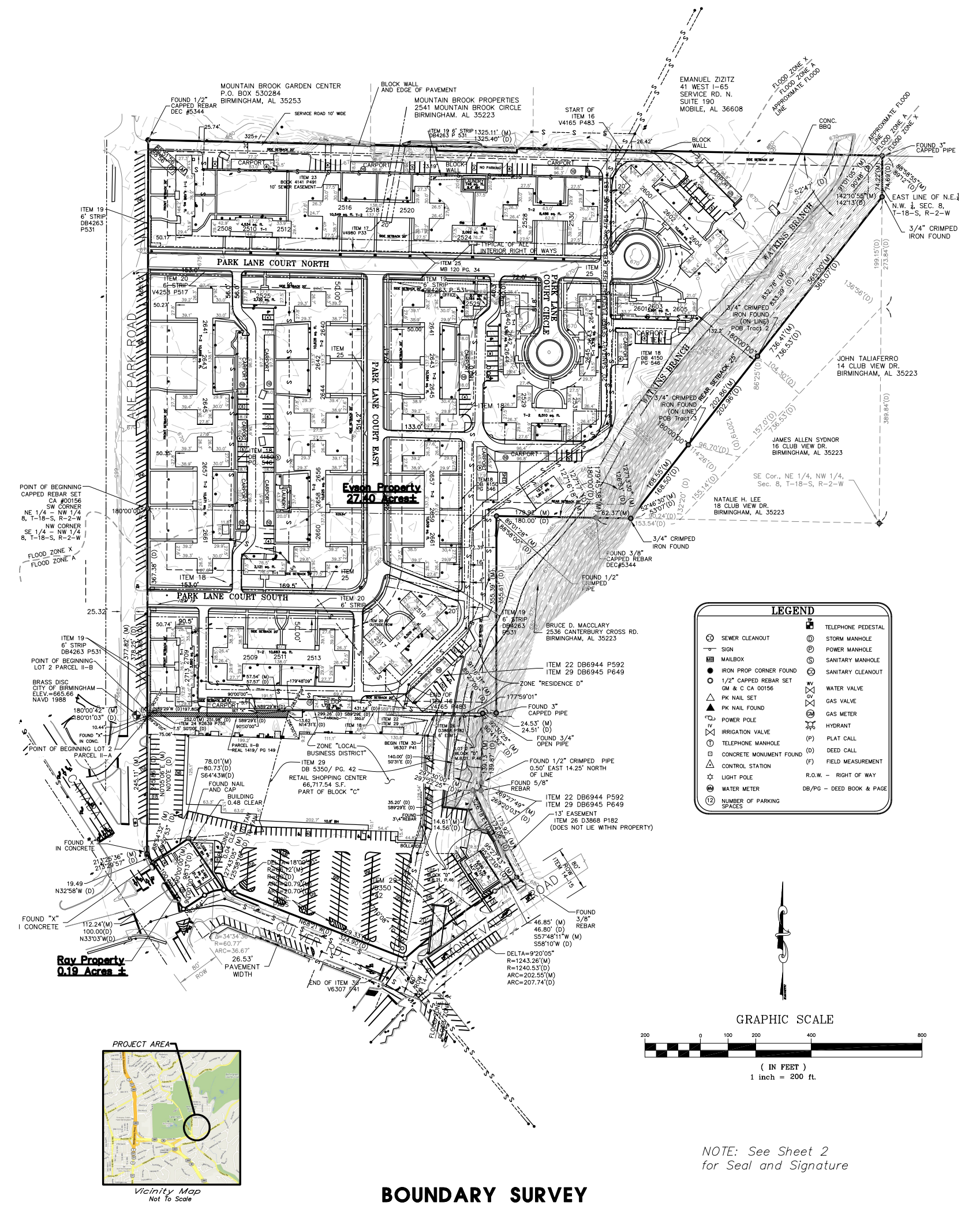


17. SIZE, ADDRESS AND TAX PARCEL IDENTIFICATION NUMBERS

- I. **Current Ownership:** The Property is currently owned by the following persons:
- Evson, Inc., an Alabama limited liability company; and
 - RC Limited, LLC, an Alabama limited liability company; and
 - Lane Parke Apartments, LLC, an Alabama limited liability company.
- II. **Size:** The Property consists of approximately 27 acres. The minimum acreage required to rezone property to a Planned United Development District pursuant to Section 129-261(d) of the City Code is four (4) acres for property covered by an approved village master plan. The Property is covered by the Village Master Plans.
- III. **Address:** The Property is currently identified by the following primary addresses:
- 2700 – 2721 Culver Road, Birmingham, Alabama 35223 (Mt. Brook Shopping Center portion of Evson Parcel)
 - 2629 Cahaba Road, Birmingham, Alabama 35223 (Ray Parcel)
 - 2525 Park Lane Court North, Birmingham, Alabama 35223 (Park Lane Apartments portion of Evson Parcel)
- IV. **Tax Parcel Identification Numbers:** The Property is currently identified by the following tax parcel identification numbers as assigned by the Jefferson County Revenue Department:
- 28-08-2-001-005.000 (Park Lane Apartments portion of Evson Parcel)
 - 28-08-2-001-006.000 (Mt. Brook Shopping Center portion of Evson Parcel)
 - 28-08-2-001-007.000 (Ray Parcel)
- V. **Mortgages:** The Property is currently subject to the following recorded mortgages:
- **Evson Parcel**
Mortgages in favor of Wells Fargo Bank, National Association:
Address: Wells Fargo Bank, National Association
Middle Market Real Estate
2907 Central Avenue, Suite 109
Homewood, Alabama 35209
Mortgage in favor of Synovus Bank:
Address: Synovus Bank
800 Shades Creek Parkway
Birmingham, Alabama 35209
 - **Ray Parcel**
Mortgage in favor of Regions Bank (as successor to AmSouth Bank):
Address: Regions Bank
520 Montgomery Highway
Vestavia Hills, Alabama 35216
Mortgage in favor of First Commercial Bank:
Address: First Commercial Bank
800 Shades Creek Parkway
Birmingham AL 35209

18. SURVEY WITH ELEVATIONS & AERIAL PHOTOGRAPH

SURVEY - SHEET ONE



Legal Description: Evson, Inc. Property

State of Alabama
Jefferson County
City of Mountain Brook

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Begin at the Southwest Corner of the Northeast Quarter of the Northwest Quarter of Section 8, Township 18 South, Range 2 West, being the Point of Beginning; thence run Northerly along the West line of said Quarter-Quarter a distance of 665.12 feet; thence right 91° 08' 04" a distance of 1325.11 feet; thence right 88° 58' 55" a distance of 74.22 feet; thence right 37° 49' 05" a distance of 736.41 feet; thence right 52° 46' 30" a distance of 62.37 feet; thence right 00° 14' 22" a distance of 179.92 feet; thence left 90° 58' 32" a distance of 355.39 feet; thence right 88° 43' 29" a distance of 24.53 feet; thence left 87° 29' 35" a distance of 139.13 feet; thence right 89° 27' 49" a distance of 14.61 feet; thence left 117° 30' 00" a distance of 175.92 feet; thence right 84° 32' 17" a distance of 46.85 feet; thence tangent to a curve to the left having a radius of 1243.26 feet and a central angle of 09° 20' 05" along the curve an arc distance of 202.55 feet; thence right 62° 49' 52" from the tangent of said curve a distance of 329.33 feet; thence tangent to a curve to the left having a central angle of 18° 00' 50" and a radius of 66.12 feet, an arc distance of 20.79 feet; thence right 52° 16' 55" from the tangent of said curve a distance of 112.24 feet; thence left 90° 00' 00" a distance of 78.01 feet; thence right 91° 05' 28" a distance of 19.49 feet; thence right 33° 25' 36" a distance of 245.11 feet; thence right 00° 00' 42" a distance of 377.82 feet to the Point of Beginning. Said parcel contains 27.40 acres more or less.

Legal Description: Ray Property

State of Alabama
Jefferson County
City of Mountain Brook

A parcel of land being situated in the Southeast Quarter of the Northwest Quarter of Section 8, Township 18 South, Range 2 West, more particularly described as follows:

Commence at the Northwest Corner of the Southeast Quarter of the Northwest Quarter of Section 8, Township 18 South, Range 2 West; thence run South along the West line of said Quarter-Quarter a distance of 377.82 feet; thence left 00° 00' 42" a distance of 245.11 feet; thence left 33° 25' 36" a distance of 19.49 feet, to the Point of Beginning; thence left 91° 05' 28" a distance of 78.01 feet; thence right 90° 00' 00" a distance of 112.24 feet; thence right 125° 40' 04" to the tangent of a curve to the left having a radius of 60.77 feet and a central angle of 34° 34' 36", along the curve an arc distance of 36.67 feet; thence from the tangent of said curve continue a distance of 45.64 feet; thence right 90° 00' 00" a distance of 100.00 feet to the Point of Beginning. Said parcel contains 0.19 acres more or less.

Schedule B - Section II Exceptions

1. Not a Matter of Survey
2. Not a Matter of Survey
3. Not a Matter of Survey
4. None Found
5. Not a Matter of Survey
6. Not a Matter of Survey
7. Not a Matter of Survey
8. Lease dated May 16, 1973 recorded in Volume 3437, Page 702 - Not a matter of survey
9. Lease dated October 23, 1980 recorded in Volume 2000, Page 853 and Volume 3880 Page 957 - Not a matter of survey
10. Memorandum of lease dated February 9, 1990 recorded in Volume 3956, Page 221. Not a matter of survey
11. Lease recorded in Instrument Number 9710/4500 - Not a matter of survey
12. Lease recorded in Instrument Number 9908/1368 - Not a matter of survey
13. Not a matter of survey
14. Map Book 21, Page 46, No restrictions or reservations found, Rights-of-Way are as shown on drawing
15. Map Book 21, Page 46; Map Book 120, Page 34; No restrictions or reservations found, Rights-of-Way are as shown on drawing
16. Easement and right-of-way to Jefferson County recorded in Volume 4165, Page 483; Map Book 31, Page 11 - As shown on survey; Declaration of Vacation recorded in Deed Book 4165, Page 486 - Blanket in Nature
17. Right-of-Way recorded in Volume 4980, Page 31. Does Not Apply; Volume 4980, Page 33 - As Shown on Survey; Volume 2514, Page 455 - Vacated sewer therefore not shown on survey
18. Right-of-Way recorded in Volume 4150, Page 546. As shown on survey
19. Right-of-Way recorded in Volume 4263, Page 531 - Right-of-Way is outside road right-of-way in several areas as shown on survey
20. Right-of-Way recorded in Volume 4258, Page 517 - Right-of-Way is 9' outside road right-of-way on the eastern end as shown on survey
21. Agreement recorded in Volume 4225, Page 162 - Not a matter of survey
22. Right-of-way recorded in Volume 6207, Page 390, Blanket in nature, and Volume 6944 Page 592 - As shown on survey
23. Easement recorded in Volume 4141, Page 491 - As shown on survey
24. Right-of-Way recorded in Volume 2639, Page 755 - As shown on survey
25. Map of street dedication recorded in Map Book 120, Page 34 -As shown on survey
26. Easement for sewer line recorded in Volume 3868 Page 182 - As shown on survey
27. Right-of-Way recorded in Volume 2514, Page 467, Volume 2514, Page 470 - Vacated sewer therefore not shown on survey
28. Easement recorded in Volume 4980, Page 33 - Does not apply
29. Right-of-way recorded in Volume 5350, Page 42 - As shown and Blanket in nature, Volume 6945, Page 649, #F-BD-4702, Alabama Power Sketch - As shown on survey
30. Right of way recorded in Volume 6307, Page 41 - As shown on survey

Boundary Survey General Notes:

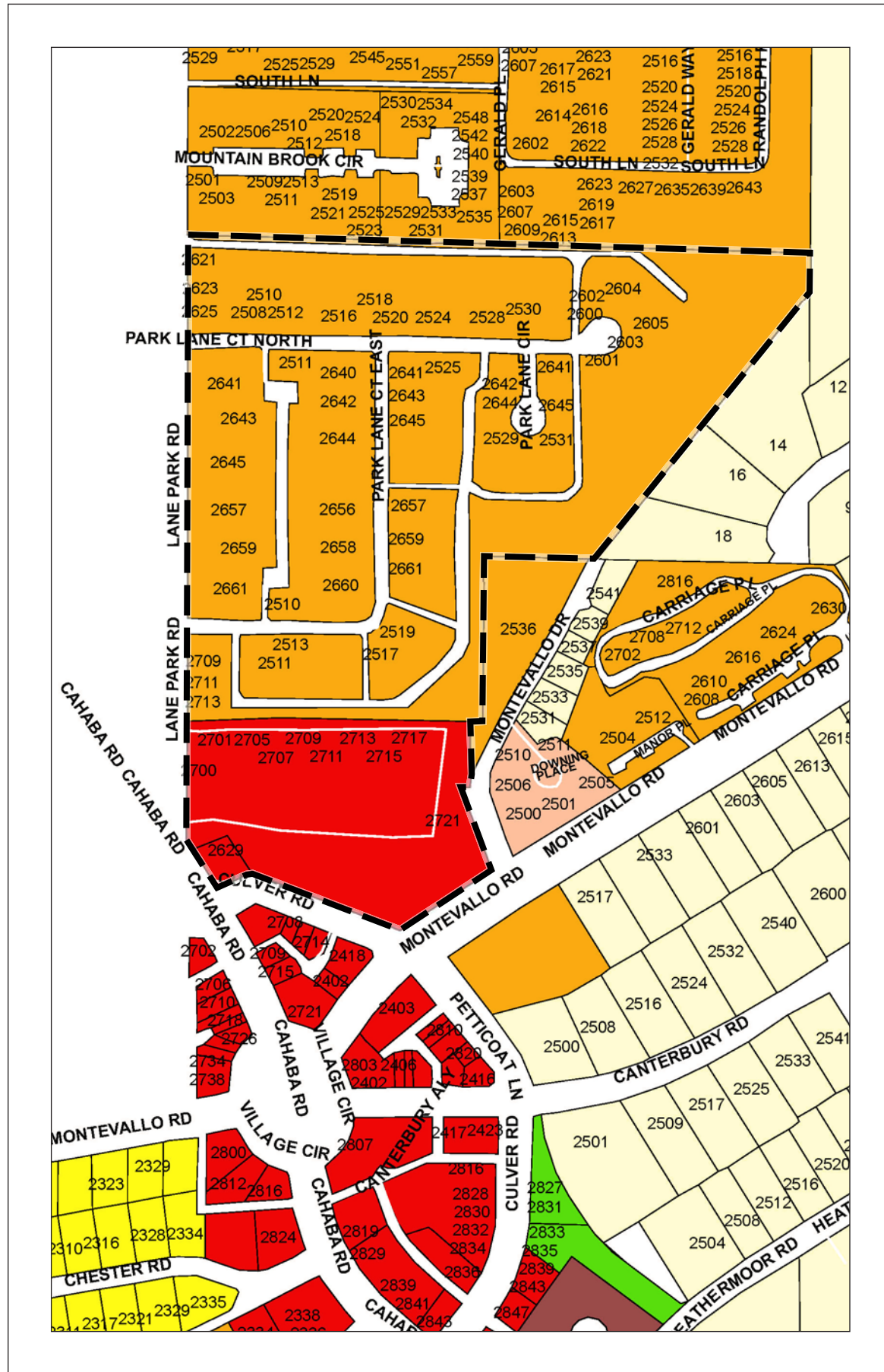
1. **Underground utilities & encroachments:** There was no attempt made to determine the location or extent of a possible encroachment beneath the surface. Underground structures and facilities that are shown were located from surface markings or graphically from drawings furnished by the utility. No excavation of underground utilities, underground mines or subsurface building foundations was made as a part of this survey.
2. **Date of survey:** February 9th, 2010 is the date of field survey and the last day the surveyor gathered data for the survey.
3. **Source of information used:** Deeds listed in exceptions and boundary survey map by Douglas Engineering, dated August 16, 1994, Deed 4234, Page 91, Deed 4448 Page 325, Deed 4447 Page 537, Deed 4447 Page 547, Deed 4447 Page 549, Jefferson County Tax Map.
4. **Signature and seal:** This survey and drawing is not valid without the signature and seal of Van Marcus Peavy, Alabama Professional Land Surveyor.
5. **Type of survey:** This is a Boundary Survey.
6. Property lies in Flood Zone A and Zone X as per community F.I.R.M. Panel #01073C0557G, effective date, September 29, 2006.
7. Setback lines based on Residence D zoning and opinion of Jerry Weems, Building Manager, City of Mountain Brook. Setback lines have changed since Lane Park development and buildings are "grandfathered" for meeting requirements.

I hereby state that all parts of this survey and drawing have been completed in accordance with the current requirements of the Standards of Practice for Surveying in the State of Alabama to the best of my knowledge, information and belief.


Van Marcus Peavy, PLS 16681 Date of Signature 5-19-10



The surrounding land use of Lane Park is generally categorized as multi-family attached residential structures to the north; single-family attached residential and single-family detached residential structures to the east; the City of Birmingham and Birmingham Botanical Gardens to the west; and village commercial to the south.



LEGEND

	CLUSTERED RESIDENTIAL
	COMMUNITY SHOPPING
	LOCAL BUSINESS DISTRICT
	RESIDENCE A DISTRICT
	RESIDENCE B DISTRICT
	RESIDENCE C DISTRICT
	RESIDENCE D DISTRICT
	RESIDENCE G DISTRICT
	RECREATION DISTRICT
	RID

LEGAL DESCRIPTION OF THE PROPERTY

State of Alabama
 Jefferson County
 City of Mountain Brook

A parcel of land being situated in the Northeast quarter of the Northwest quarter and the Southeast quarter of the Northwest quarter of Section 8, Township 18 South, Range 2 West, more particularly described as follows:

Begin at the Southwest Corner of the Northeast Quarter of the Northwest Quarter of Section 8, Township 18 South, Range 2 West; being the Point of Beginning; thence run Northerly along the West line of said Quarter - Quarter a distance of 665.12 feet; thence right 91°-08'-04" a distance of 1325.11 feet; thence right 88°-58'-55" a distance of 74.22 feet; thence right 37°-49'-05" a distance of 736.41 feet; thence right 52°-46'-30" a distance of 62.37 feet; thence right 00°-14'-22" a distance of 179.92 feet; thence left 90°-58'-32" a distance of 355.39 feet; thence right 88°-43'-29" a distance of 24.53 feet; thence left 87°-29'-35" a distance of 139.13 feet; thence right 89°-27'-49" a distance of 14.61 feet; thence left 117°-30'-00" a distance of 175.92 feet; thence right 84°-32'-17" a distance of 46.85 feet; thence tangent to a curve to the left having a radius of 1243.26 and a central angle of 9°-20'-05" along the curve an arc distance of 202.55 feet; thence right 62°-49'-52" from the tangent of said curve a distance of 329.33 feet; thence tangent to a curve to the left having a central angle of 18°-00'-50" and a radius of 66.12 feet an arc distance of 20.79 feet; thence left 2°-03'-01" to the tangent of a curve to the left having a central angle of 34°-34'-36" and a radius of 60.77 feet, an arc distance of 36.67 feet; thence continue from the tangent of said curve a distance of 45.64 feet; thence right 90°-00'-00" a distance of 119.49 feet; thence right 33°-25'-36" a distance of 245.11 feet; thence right 0°-00'-42" a distance of 377.82 feet to the Point of Beginning.

Said Parcel contains 27.59 acres more or less.

LEGAL DESCRIPTION OF THE EVSON PARCEL

State of Alabama
 Jefferson County

A parcel of land being situated in the Northeast quarter of the Northwest quarter and the Southeast quarter of the Northwest quarter of Section 8, Township 18 South, Range 2 West, more particularly described as follows:

Begin at the Southwest Corner of the Northeast Quarter of the Northwest Quarter of Section 8 Township 18 South Range 2 West being the Point of Beginning; thence run Northerly along the West line of said Quarter - Quarter a distance of 665.12 feet; thence right 91°-08'-04" a distance of 1325.11 feet; thence right 88°-58'-55" a distance of 74.22 feet; thence right 37°-49'-05" a distance of 736.41 feet; thence right 52°-46'-30" a distance of 62.37 feet; thence right 00°-14'-22" a distance of 179.92 feet; thence left 90°-58'-32" a distance of 355.39 feet; thence right 88°-43'-29" a distance of 24.53 feet; thence left 87°-29'-35" a distance of 139.13 feet; thence right 89°-27'-49" a distance of 14.61 feet; thence left 117°-30'-00" a distance of 175.92 feet; thence right 84°-32'-17" a distance of 46.85; thence tangent to a curve to the left having a radius of 1243.26 and a central angle of 9°-20'-05" along the curve an arc distance of 202.55 feet; thence right 62°-49'-52" from the tangent of said curve a distance of 329.33 feet; thence tangent to a curve to the left having a central angle of 18°-00'-50" and a radius of 66.12 feet an arc distance of 20.79 feet; thence right 52°-16'-55" from the tangent of said curve a distance of 112.24 feet; thence left 90°-00'-00" a distance of 78.01 feet; thence right 91°-05'-28" a distance of 19.49 feet; thence right 33°-25'-36" a distance of 245.11 feet; thence right 0°-00'-42" a distance of 377.82 feet to the Point of Beginning.

Said parcel contains 27.40 acres more or less.

LEGAL DESCRIPTION OF THE RAY PARCEL

State of Alabama
 Jefferson County

A parcel of land being situated in the Southeast quarter of the Northwest quarter of Section 8, Township 18 South, Range 2 West, more particularly described as follows:

Begin at the Northwest corner of the Southeast quarter of the Northwest quarter of Section 8 Township 18 South Range 2 West; thence run south along the west line of said quarter - quarter a distance of 377.82 feet; thence left 00°-00'-42" a distance of 245.11 feet; thence left 33°-25'-36" a distance of 19.49 feet, to the Point of Beginning; thence left 91°-05'-28" a distance of 78.01 feet; thence right 90°-00'-00" a distance of 112.24 feet; thence right 125°-40'-04" to the tangent of a curve to the left having a radius of 60.77 feet and a central angle of 34°-34'-36" along the curve an arc distance of 36.67 feet; thence from the tangent of said curve continue a distance of 45.64 feet; thence right 90°-00'-00" a distance of 100.00 feet; to the Point of Beginning.

Said parcel contains 0.19 acres more or less.



STEVEN A. BRICKMAN
ATTORNEY AT LAW
(205) 936-5171
sbrickman@sirote.com

SIROTE & PERMUTT
A PROFESSIONAL CORPORATION

February 8, 2012

To Whom It May Concern:

To our knowledge and based upon our review of title due diligence obtained by the Project Owner (as defined in the PUD Application), we confirm that the Property (as defined in the PUD Application) does not currently appear to be subject to any covenants or restrictions of record except as set forth below:

A restriction encumbering the Ray Parcel contained in that certain Deed recorded in Book 1059, Page 872 in the Office of the Judge of Probate of Jefferson County, Alabama providing that no petroleum products shall be advertised, stored, sold or distributed from the premises for a period of twenty years following the date of the Deed. The date of the Deed is April 17, 1974, a copy of which is attached hereto.

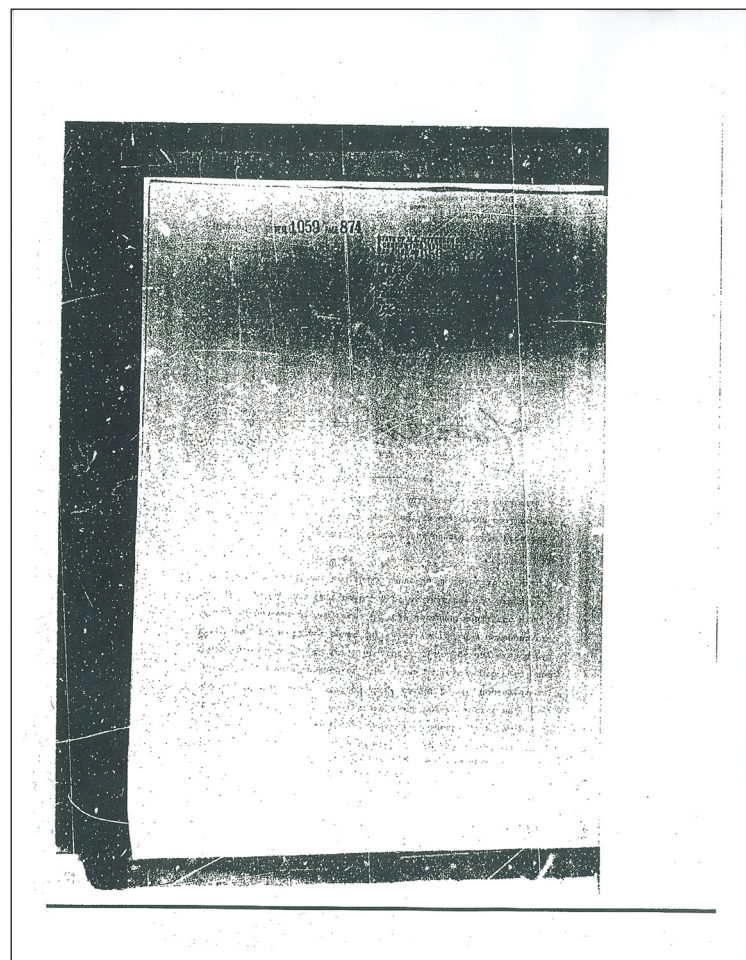
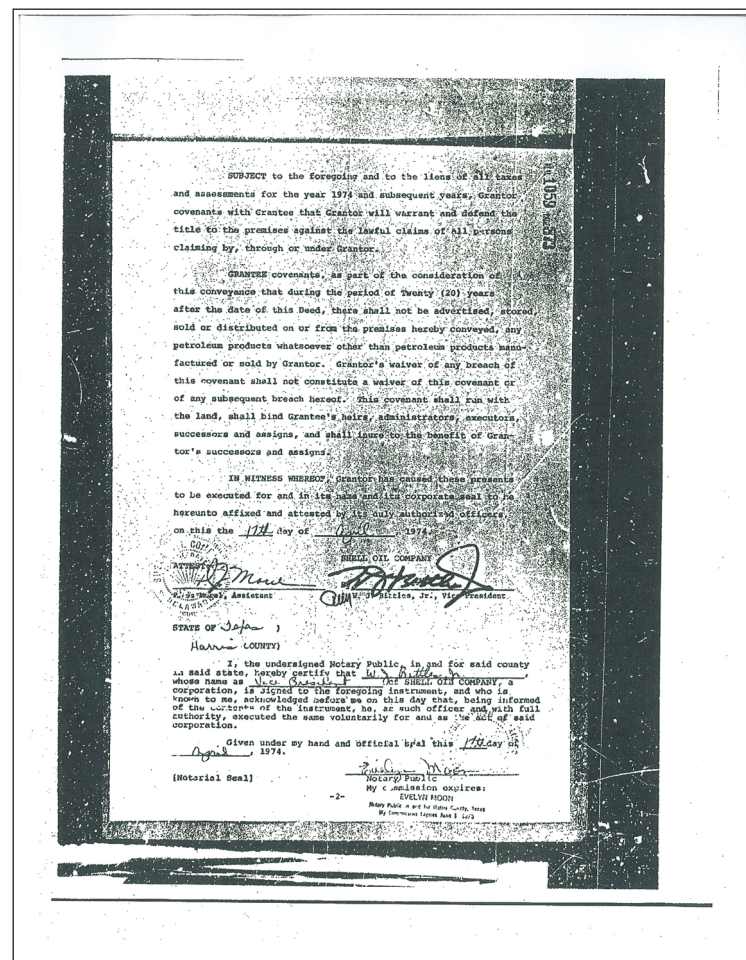
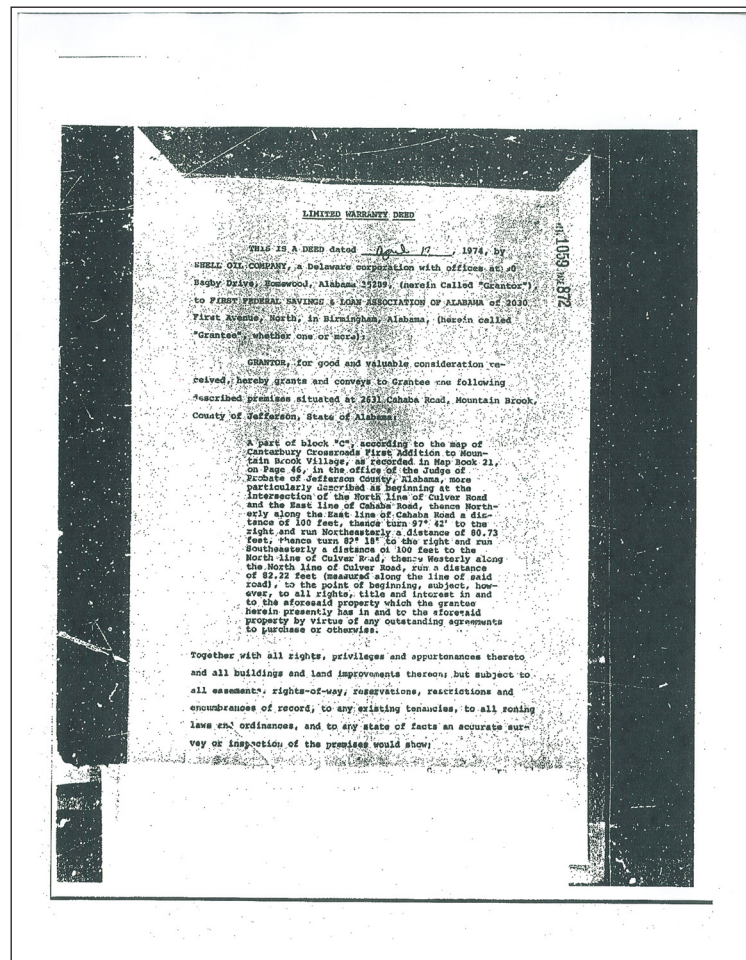
Sirote & Permutt, P.C.

By: *Steven A. Brickman*
Steven A. Brickman
FOR THE FIRM

SAB/pta

DOCSBHM17020292

Law Offices and Mediation Center
2311 Highland Avenue South Birmingham, Alabama 35205
Post Office Box 87027 Birmingham, Alabama 35205-8727
TELEPHONE | 205.936.5100 FAX | 205.936.5101 URL | http://www.sirote.com
Birmingham | Huntsville | Mobile



600 20th Street North
Birmingham, Al. 35203
205 251 2871
facsimile 205 226 9280
Sender's direct dial 205 380 4164

Land Title Company of Alabama

April 16, 2012

Mr. Steve Brickman
Sirote & Permutt, P.C.
2311 Highland Avenue South
Birmingham, Al. 35205

Re: Evson Address Research/Certification of Owners

Dear Steve:

Land Title Company certifies that, as of April 5, 2012, the list of names and addresses attached hereto as Exhibits "A" and "B" is a true and correct list of the names and addresses of all property owners whose property lies within 500 feet of any portion of the Property. The Property is defined as that property identified as Parcel ID. Numbers 28-08-2-001-005; 006; 007 and 008 as shown on the records of the Tax Assessor of Jefferson County, Alabama.

For purposes of identifying the property owners, the property owner is considered to be the person who is shown as the owner of such parcel according to the records of the Tax Assessor of Jefferson County, Alabama.

Sincerely,
Susan R. Gannett
Susan R. Gannett, Senior Vice President

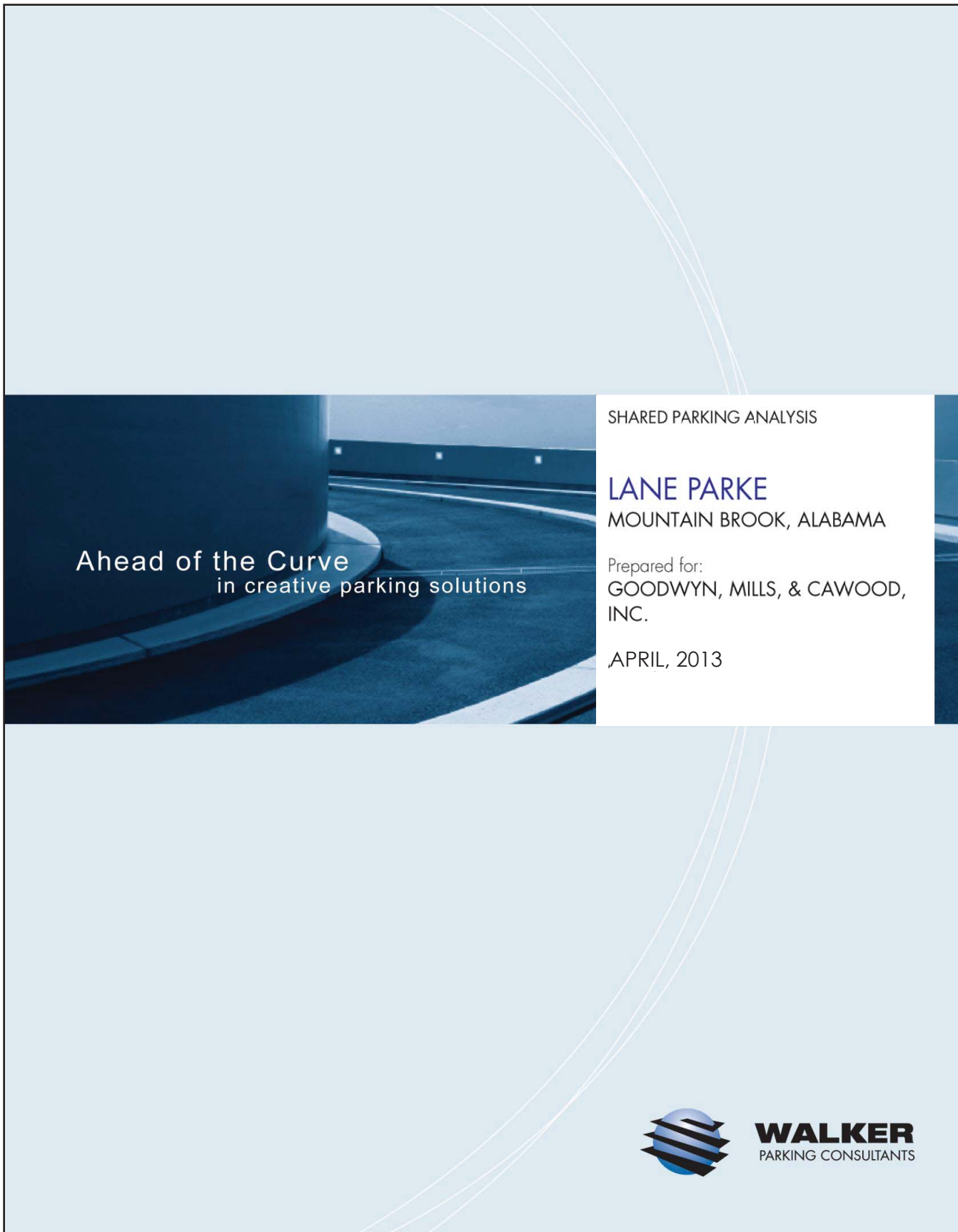


NAMES & ADDRESSES OF ADJOINING PROPERTY OWNERS


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A full copy of the Shared Parking Analysis has been submitted as a part of this PUD Application and is available for review at Mountain Brook City Hall.



A full copy of the Traffic Impact Study has been submitted as a part of this PUD Application and is available for review at Mountain Brook City Hall.

MEMORANDUM		
LANE PARK PARKING DEMAND		
PAGE 1		
DATE:	June 10, 2013	6602 East 75th Street, Suite 210 Indianapolis, IN 46250
TO:	John Knutsson	Office: 317.842.6890 Fax: 317.577.6500 www.walkerparking.com
COMPANY:	Daniel Corporation	
ADDRESS:		
CITY/STATE:		
CC:		
HARD COPY TO FOLLOW: NO		
FROM:	Jeff Colvin	
PROJECT NAME:	Lane Parke	
PROJECT NUMBER:	15-1901.10	
SUBJECT:		

Per your direction, I have looked at the shared parking model if the following changes are made to the development plan:

Elimination of the 11,157 sq. ft. pharmacy
Adding 6,500 sq. ft. of bank use

The resulting peak parking demand would be 942 spaces on a weekend, 898 spaces on a weekday. This represents a reduction of 51 spaces on a weekend and 42 spaces on a weekday from the base shared parking model.


Detailed peak parking demand tables are outlined in the following tables, broken out for weekday and weekend.

**TRAFFIC
IMPACT
STUDY**

Lane Parke


Mountain Brook, Alabama

Prepared for:



GOODWYN, MILLS AND CAWOOD, INC.

Prepared by:



**SKIPPER
CONSULTING INC.**

September 2009
Revision 1: May 2010
Revision 2: January 2012
Revision 3: March 2012
Revision 4 : June 2013



SKIPPER Consulting, Inc.

3644 Vann Road, Suite 100
 Birmingham, Alabama 35235
 Phone (205) 655-8855 Fax (205) 655-8825

July 25, 2013
 CORRECTED August 9, 2013

John L. Knutsson
 Vice President
 Daniel Corporation
 3660 Grandview Parkway
 Suite 100
 Birmingham, Alabama 35243

RE: Lane Parke

Dear John:

The purpose of this letter is to provide an analysis of the potential changes to the results of our most recent traffic study for the Lane Park development (dated June 11, 2013) based on proposed changes to the development plan. A comparison of the development plan included in the report and the proposed development follows:

<u>Land Use</u>	<u>Development Plan in Report</u>	<u>Proposed Development Plan</u>
Retail	49,715 sq.ft.	47,415 sq.ft.
Pharmacy	11,157 sq.ft.	--
Restaurant (Fast/Casual)	6,000 sq.ft.	3,400 sq.ft.
Restaurant (Quality)	16,500 sq.ft.	12,500 sq.ft.
Grocery	28,300 sq.ft.	28,300 sq.ft.
Hotel	100 rooms	100 rooms
Bank	4,340 sq.ft.	12,340 sq.ft.
Apartments	276 units	276 units

A comparison of the total external trip generation of the two proposed land use plans is shown in the following table.

August 9, 2013

**Table
 Trip Generation Comparison**

Land Use	External Trip Generation								
	Weekday			AM Peak Hour			PM Peak Hour		
	In	Out	Total	In	Out	Total	In	Out	Total
Current Report	4669	4669	9338	168	177	345	504	433	938
Proposed Changes	4452	4452	8904	182	187	368	506	445	951

As shown in the preceding table, the proposed land use generates approximately 5% less traffic than the land use included in the latest traffic impact study on a daily basis.

During the a.m. peak hour, the proposed land will generate approximately 7% more traffic than the land use included in the current traffic impact study; however, there are no roadway capacity issues during the a.m. peak hour, and therefore it is anticipated that the change in land use would not cause the need for additional roadway improvements to accommodate the a.m. peak hour of traffic flow.

During the p.m. peak hour, the proposed land use would generate approximately 1% more traffic than the land use included in the traffic impact study. This is negligible and will not cause any change in the analysis results.

Based on the information presented in this letter, it is the opinion of Skipper Consulting that the proposed changes in land use for Lane Parke would not change the results of the traffic impact study.

Sincerely,


 Richard L. Caudle, P.E.



EXECUTIVE SUMMARY
LANE PARKE TRAFFIC IMPACT STUDY
UPDATED JUNE, 2013

Skipper consulting ("Skipper") has updated its original traffic impact study ("Study") dated January, 2012, for the Lane Parke redevelopment in Mountain Brook, Alabama submitted in connection with the previously approved planned unit development zoning ("PUD") for Lane Parke ("Development").

After a thorough review of the revised density and phasing of the Development, Skipper now recommends modifications to the external road improvements previously proposed for the Development.

The proposed site access system will improve the traffic flow to and from the Development for a variety of reasons. By reducing the number of access points to the public roadway system drivers traversing the public roadways adjacent to the Development will experience fewer delays which arise from the numerous entry and exit points and will benefit from streamlines traffic flow in and out of the Development. The internal roads which will be constructed within the Development provide, among other benefits, a direct link between Montevallo Road and Lane Park Road and should decrease the traffic at the intersection of Cahaba Road, Lane Park Road and U.S. 280 ramps at heavy traffic periods as the public utilizes these internal roads, thereby alleviating traffic at the busier intersections.

The reduced density of the Development, and thus reduced vehicular traffic, produces less strain on the surrounding roadways. Based on the updated study, the following improvements are designed to improve the traffic flow in and around Lane Parke:

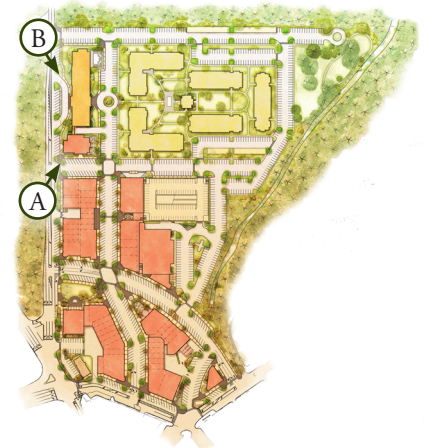
- i. Widening of approximately 440 lf of Lane Park Road to allow for the construction of a left turn lane from Lane Park Road southbound onto Culver road;
- ii. Widening Culver Road to a three lane cross-section which will allow for an improved alignment of traffic crossing from Culver Road onto U.S. 280 ramp;
- iii. Restripe Montevallo Road to provide a left turn lane into Jemison Lane. And
- iv. Adding a phase to the traffic signal at the intersection of Cahaba Road/Lane Park Road/Culver Road/ U.S. 280 ramps to allow for a protective green arrow for traffic exiting U.S. 280 to go north on Lane Park Road and Cahaba Road.

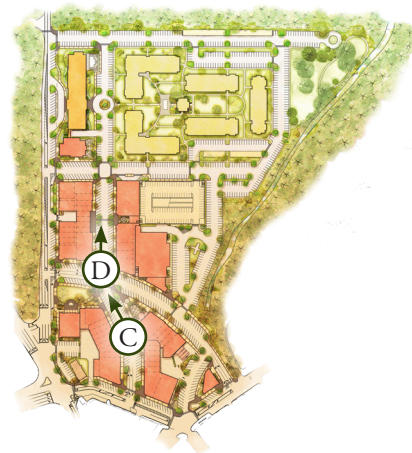
When the proposed improvements are in place at each stage of construction, the Study demonstrates the traffic at the surrounding intersections will operate at acceptable levels of service during both the a.m. and p.m. peak hours of traffic flow. Likewise, the Study demonstrates that all roadway segments studied will operate at acceptable levels of service on a daily basis. While the left turn exiting the site onto Montevallo Road is projected to continue to operate at a level of service "E" during the p.m. peak hour, this is unchanged from the current level of service. The Study also demonstrates that the projected queues at all intersections do not block any other adjacent major intersections.



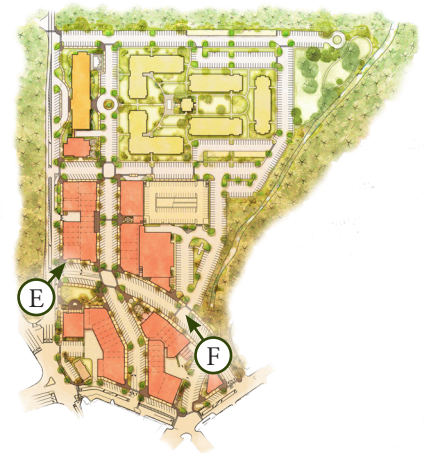
(A) View of the proposed restaurant and inn looking north along Lane Park Road.

(B) View of the inn looking south along Lane Park Road.





- Ⓒ View of the proposed retail at the northwest intersection Main Street and Jemison Lane.
- Ⓓ View north along Main Street of the proposed retail and office/commercial space.

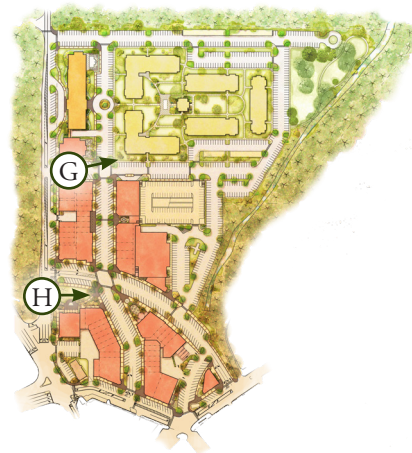


- Ⓔ View of the proposed retail at the northeast intersection Lane Park Road and Jemison Lane.
- Ⓕ View of the proposed grocery looking northwest along Jemison Lane.





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- G View of the proposed residential space at the corner of Main Street and Park Lane Court South.
- H View east along Jemison Lane of the proposed Village Green and office/commercial space.



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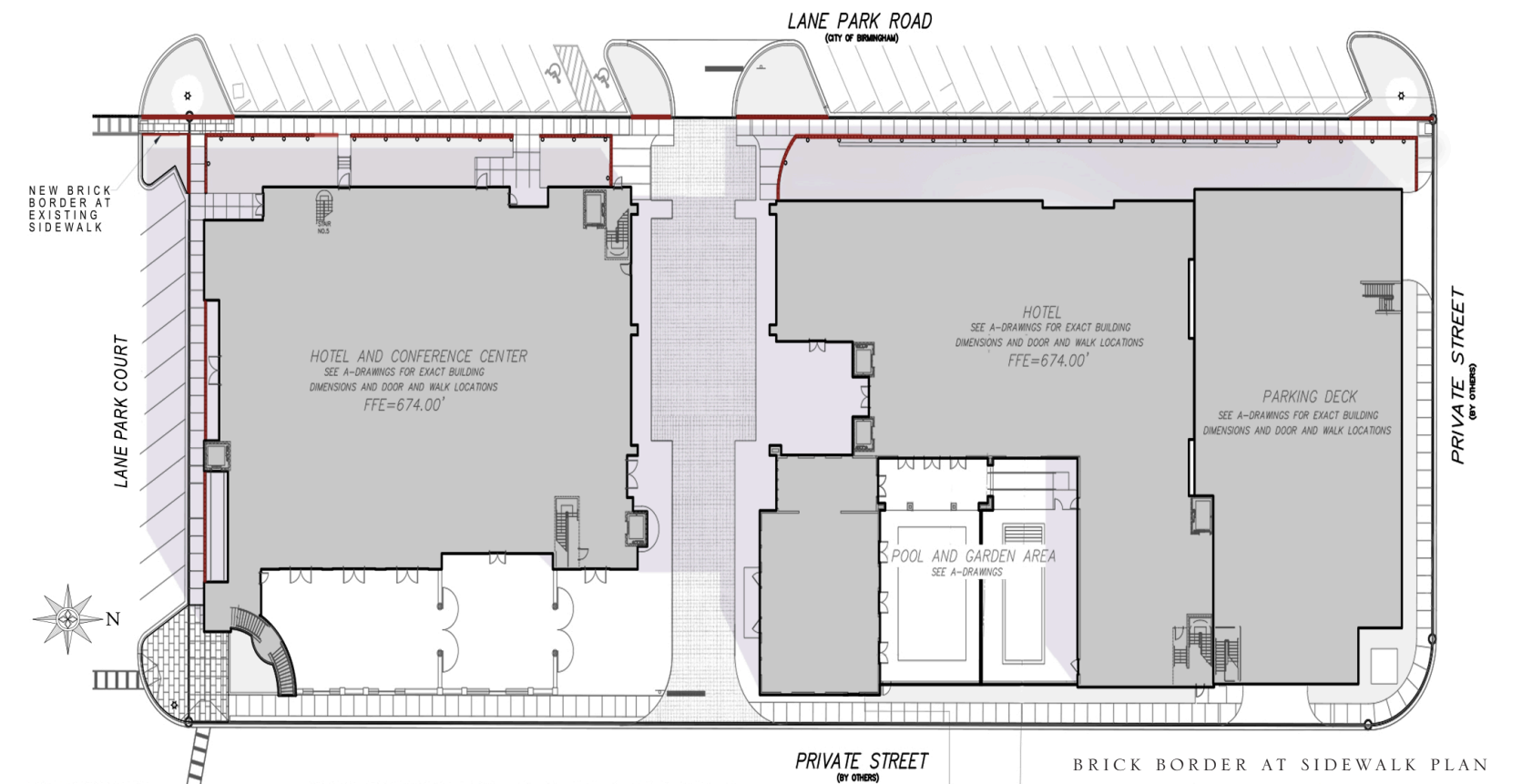
Sidewalks

- The sidewalks running along the western side of the Inn/Commercial Use Area abutting Lane Park Road and along the southern side of the Inn/Commercial Use Area abutting Lane Parke Court may be paved in concrete with brick bands, which brick bands shall be in substantially the form and design shown in Diagram 1 attached hereto, and shall be located where indicated in red on Diagram 1.
- The sidewalks running along the northern and eastern sides of the Inn/Commercial Use Area may be paved in concrete, and neither brick bands nor the scored running band pattern shall be required.

Lighting

- The following street lighting shall be permitted along the streets abutting the Inn/Commercial Use Area:
 - Traditional acorn styled street lights in substantially the form and design shown in Diagram 2 attached hereto must be used at each of the three (3) locations highlighted in yellow on Diagram 2 (the “Acorn Lighting”); and
 - Mushroom styled street lights in substantially the form and design shown in Diagram 3 attached hereto must be used in the locations highlighted in yellow on Diagram 3 attached hereto (the “Mushroom Lighting”); and
 - The Acorn Lighting and the Mushroom Lighting shall together provide 1.5 foot candles of light along Lane Park Road in the Inn/Commercial Use Area.

DIAGRAM 1



EXISTING CONDITIONS AT SIDEWALK



EXISTING CONDITIONS AT SIDEWALK



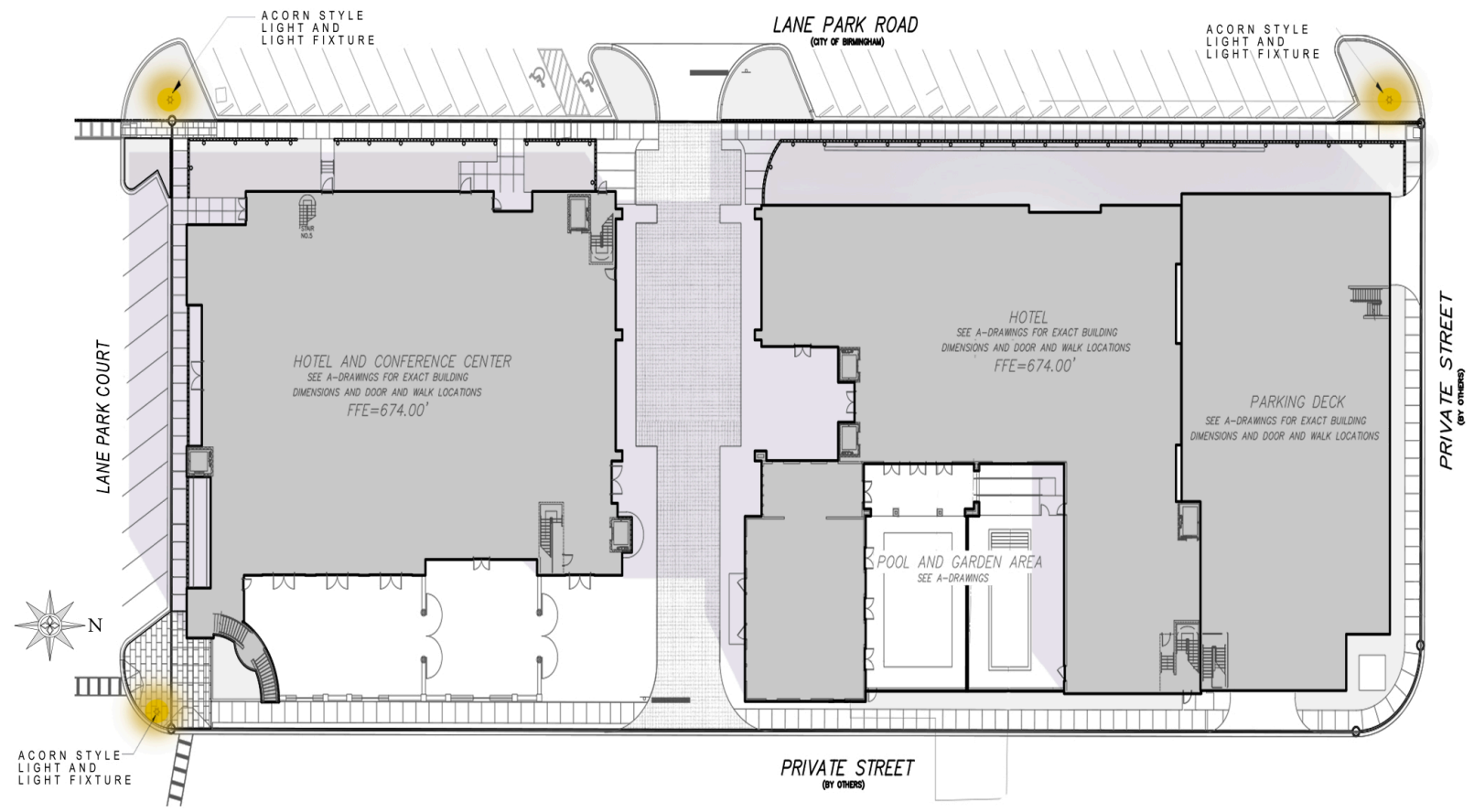
ADDED BRICK BORDER AT SIDEWALK



ADDED BRICK BORDER AT SIDEWALK



DIAGRAM 2

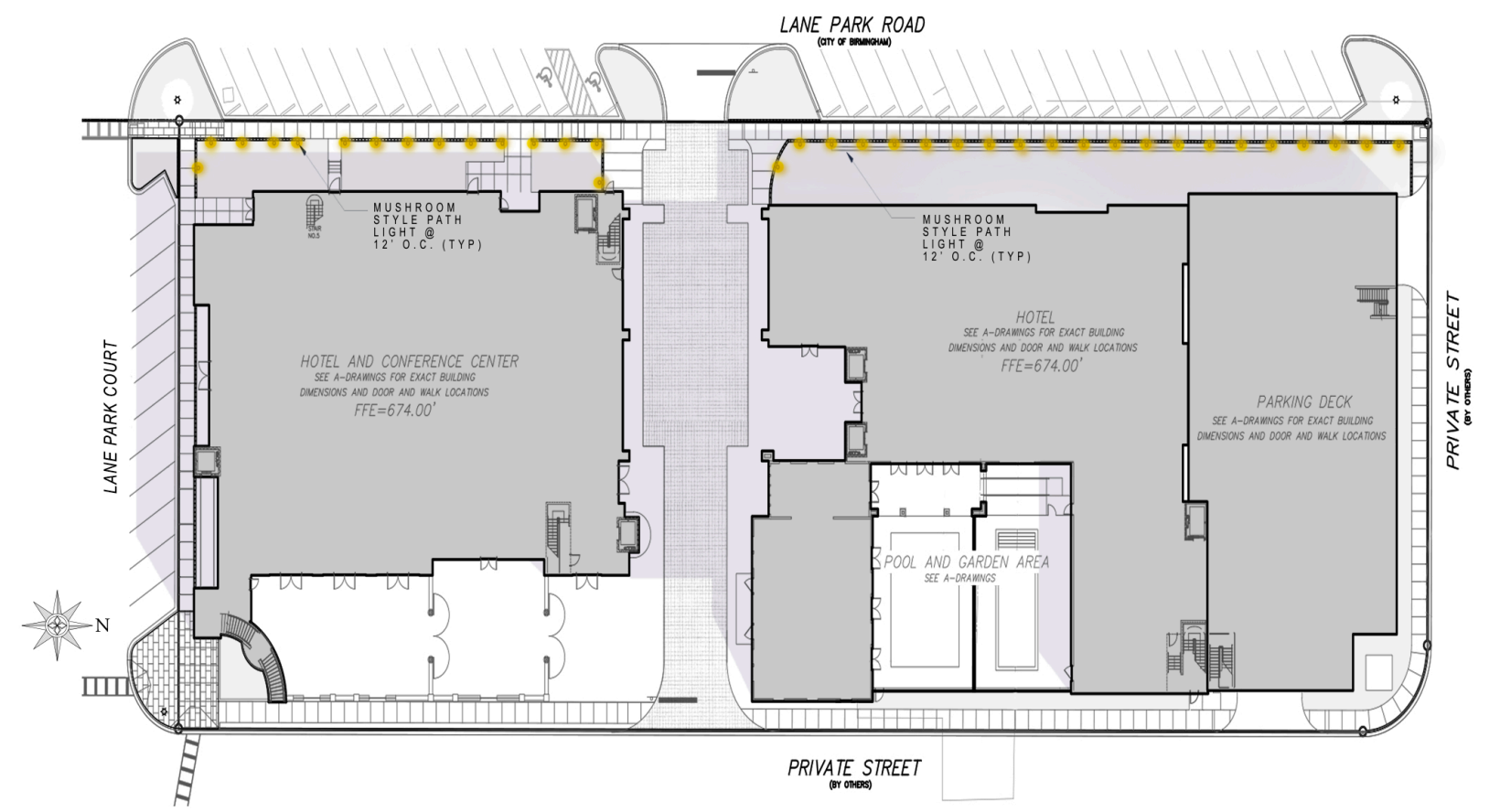


ACORN LIGHTING FIXTURE PLAN

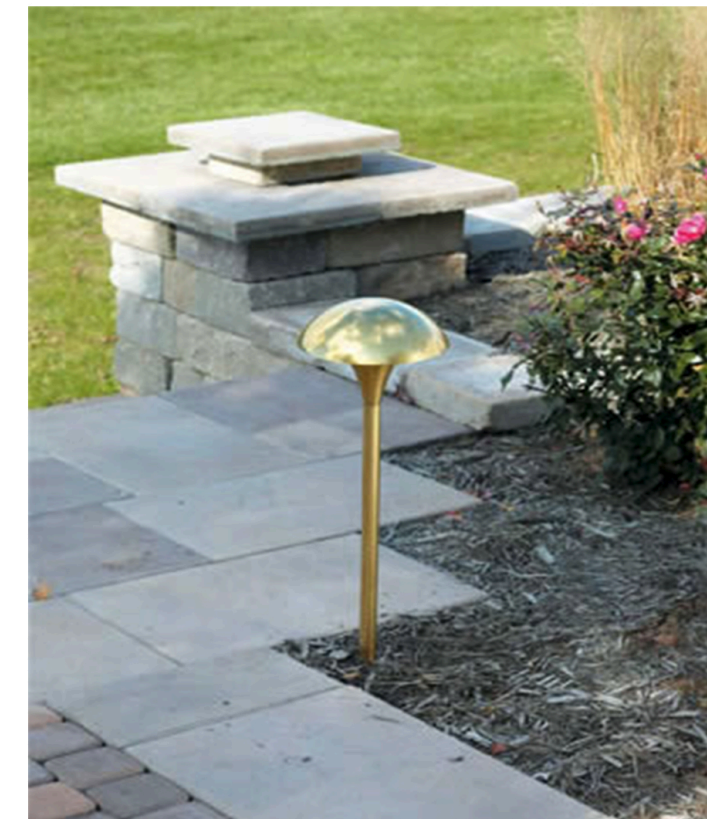


ACORN LIGHTING FIXTURE

DIAGRAM 3

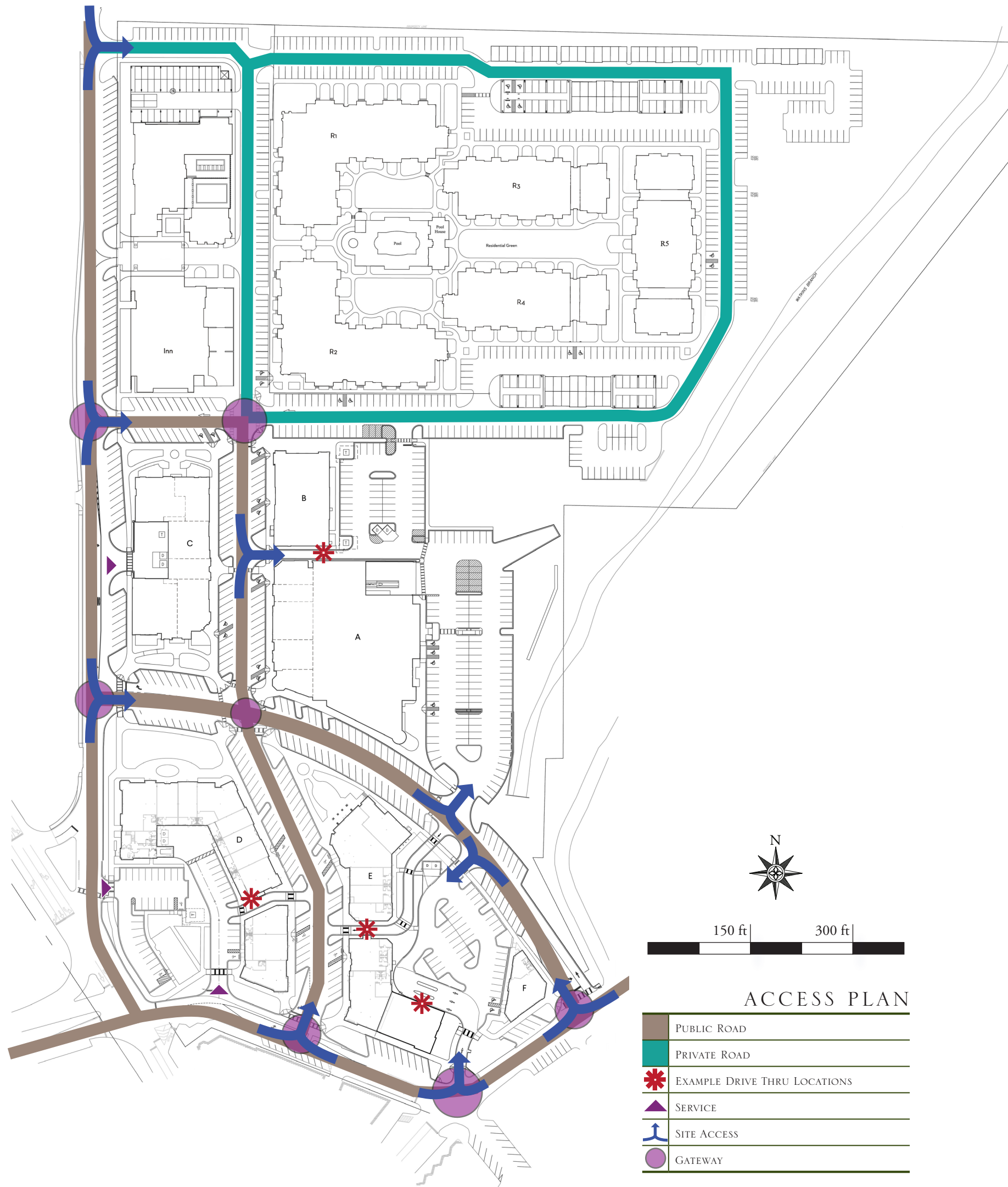


"MUSHROOM" LIGHTING FIXTURE PLAN



EXAMPLE "MUSHROOM" PATH LIGHTING FIXTURE (COLOR AND FINISH MAY VARY)





ACCESS PLAN

- PUBLIC ROAD
- PRIVATE ROAD
- ✱ EXAMPLE DRIVE THRU LOCATIONS
- ▲ SERVICE
- └┘ SITE ACCESS
- GATEWAY

MINDFUL OF THE PAST, LOOKING TO THE FUTURE

