



L A N E P A R K E

John Evans

Evson, Inc.



L A N E P A R K E

Doug Neil  
Daniel Corporation

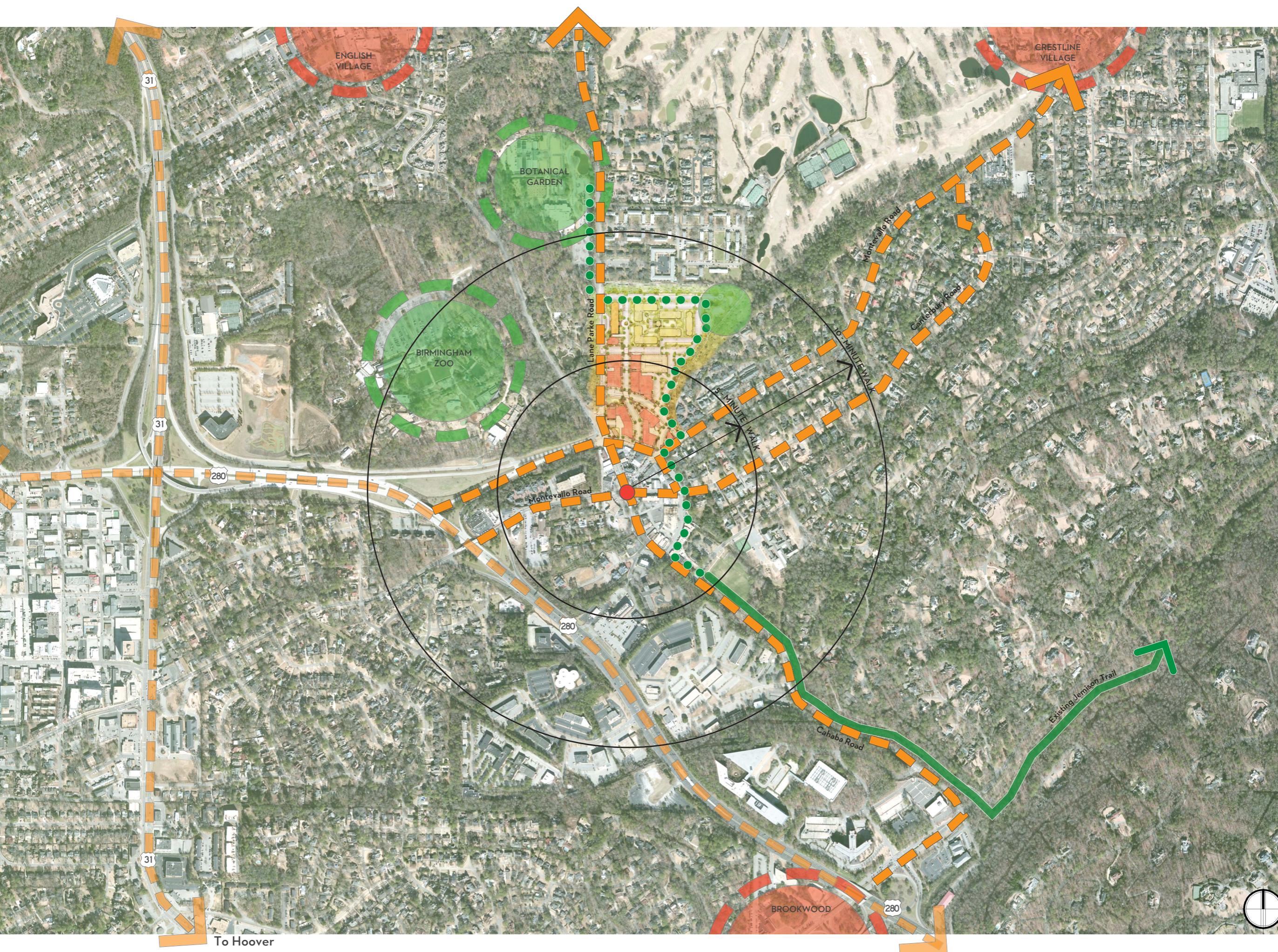
# Daniel Corporation

- ◆ Leader in Top-Quality, Mixed-Use Developments
- ◆ Successful Track Record of Projects That Serve Communities
- ◆ Economic Foundation to Deliver a Project of This Complexity
- ◆ Respectful of the Unique Character of Mountain Brook
- ◆ Mindful Mountain Brook Village is Front Door of Our Community
- ◆ One of the Best Mixed-Use Sites in the Southeast
- ◆ Current Plan Incorporates Community Input

# Lane Parke Plan

- ◆ RESPECTS the Village Master Plan
- ◆ REDUCES Scale of Commercial Component (by 22%)
- ◆ COMPREHENSIVE approach to the entire property

Jeffrey Brewer, AIA  
Goodwyn, Mills & Cawood, Inc.



ENGLISH VILLAGE

CRESTLINE VILLAGE

BOTANICAL GARDEN

BIRMINGHAM ZOO

BROOKWOOD

31

31

280

280

280

Lane Parke Road

Montevallo Road

Montevallo Road

Canterbury Road

Cahaba Road

5-MINUTE WALK

10-MINUTE WALK

Existing Jemison Trail

To Hoover



# Mountain Brook Village

## *Existing Conditions*



- ◆ AGING Commercial / Retail

- ◆ 71,529 sf of retail shops

- ◆ 7,000 sf of office space

- ◆ OBSOLETE Apartments

- ◆ 276 units

- ◆ INEFFICIENT pedestrian & traffic plan

- ◆ PRONE to flooding

- ◆ UNSUSTAINABLE





L A N E P A R K E

*Planned Unit  
Development*





# LANE PARK E

*Retail – 166,000 sf*

*Office – 30,000 sf*

*Inn – 99,000 sf (100 rooms)*

*Residential – 276 Flats*





# LANE PARK E

- ◆ FOLLOWS Village Master Plan
- ◆ SMALLER Commercial Area (22%)
- ◆ COMPREHENSIVE Approach





# Following the Master Plan

# Village Master Plan Comparison

VILLAGE MASTER PLAN SPECIAL POLICY AREA 1 - POLICY GOALS	COMPLIANCE	LANE PARKE PUD PLAN
<ul style="list-style-type: none"> <li>Any future development [of Park Lane Apartments] should occur through a Residential PUD.</li> <li>Areas to the south (the Mountain Brook Shopping Center) may be included in the PUD, and the transition line between the commercial areas of the plan and the residential portion of the plan may be adjusted, provided that (i) retail uses be focused toward Culver, with office and service uses oriented further off of Culver and (ii) the project provide enhanced vehicle and pedestrian connections to the Village.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The Lane Parke PUD Plan and Application was conceived and submitted as a direct result of the recommendations of the Village Master Plans.</li> <li>Per the policy, the commercial district has been extended, with retail uses focused towards Culver with transitions into office, service and residential uses toward the north end of the property. The plan includes a new street and new sidewalks that provide direct vehicular and pedestrian access to the Village.</li> </ul>
<ul style="list-style-type: none"> <li>Higher scale buildings may be appropriate, provided that no buildings shall exceed 6 stories or 72' to the eave or cornice line.</li> </ul>	✓	<ul style="list-style-type: none"> <li>No buildings exceed 4 stories or 46' to the eave.</li> </ul>
<ul style="list-style-type: none"> <li>Buildings shall use the design standards of the Village Overlay District.</li> </ul>	✓	<ul style="list-style-type: none"> <li>In addition to using the principles of the Village Overlay Standards, Lane Parke is governed by a strict, form and style-based pattern book.</li> </ul>
<ul style="list-style-type: none"> <li>Buildings in the 4 to 6 story range may be permitted in exchange for civic and open space that is a focal point of the plan, with taller buildings fronting on these open spaces. Such buildings should be placed with consideration of minimizing impacts on Lane Park Road.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The 4 story buildings, which are the residential apartments and the Inn, are located in the Northern quadrant of the site (set back from Lane Park Road). The buildings face a significant and focal residential park with pedestrian access to Woodland Park.</li> </ul>
<ul style="list-style-type: none"> <li>2 and 3 story buildings should be used in areas of transition with respect to the Village.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The buildings closest to Culver Road are 1 story buildings</li> <li>Retail Buildings along Lane Park Road are 1 or 2 stories</li> <li>The hotel is a 4 story building. Located in the Northern section of the property, furthest from the Village, and set back from Lane Parke Road</li> </ul>
<ul style="list-style-type: none"> <li>The Building and Development regulating Plan permits 2 story buildings along Culver Road and 3 story buildings further north.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The buildings closest to Culver Road are 1 and 2 story buildings</li> </ul>
<ul style="list-style-type: none"> <li>Buildings in the 4 to 6 story range should be limited so that they are located only to create landmark architectural features and add variety to the building scale and should only be permitted for up to 25% of the total footprint of the buildings.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The 4 story apartments and the Inn represent 25% of the total building footprints.</li> </ul>
<ul style="list-style-type: none"> <li>The plan should create significant green space, including approximately 200 square feet per dwelling unit.</li> <li>Green spaces should be designed at highly visible locations as focal points for the site.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The residential apartment buildings face a significant and focal residential park and are connected to Woodland Park, which quadruples the recommended size of open space at more than 800sf per dwelling unit.</li> <li>The Village Green is visible from Lane Parke Road, Jemison Lane and Main Street</li> </ul>
<ul style="list-style-type: none"> <li>Discrete and small parking areas should be used that are located away from Lane Park Road, away from the Village center, and away from internal streetscapes. Any parking structures shall be directed away from important streetscapes and disguised with building facades similar to neighboring buildings.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The plan utilizes primarily on-street parking, with discreet parking areas located away from the Village and primary internal streets.</li> <li>The one parking structure is centrally located away from the Village Center and away from internal streetscapes. The façade treatment will be governed by the pattern book.</li> </ul>
<ul style="list-style-type: none"> <li>No more than 15% of street frontage areas should be for driveways, drive aisles or parking areas.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The plan complies with this policy, except where sewer and storm water easements prohibit the construction of buildings.</li> </ul>
<ul style="list-style-type: none"> <li>The project should introduce stronger connections to the Village, as follows               <ul style="list-style-type: none"> <li>2 vehicular connections to Lane Park Road, and</li> <li>1 vehicular connection to the south with a second pedestrian passage.</li> </ul> </li> </ul>	✓	<ul style="list-style-type: none"> <li>The plan introduces new road and pedestrian connections:               <ul style="list-style-type: none"> <li>3 vehicular connections at Lane Park Road</li> <li>2 vehicular connections to the south (Montevallo Road and Culver Road) with pedestrian passage</li> </ul> </li> </ul>
<ul style="list-style-type: none"> <li>Need at least one stub connection from Park Lane Apartments to the north that aligns with the most significant street connection to the Village.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The plan complies with this suggestion by creating the north-south main street allowing vehicular and pedestrian access from the Village to the north.</li> </ul>
<ul style="list-style-type: none"> <li>All internal streets should have enhanced pedestrian access.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The plan complies with this policy. The plan has enhanced the pedestrian access to and from the site. Broad sidewalks are part of the plan. The plan allows movement from the north end of the property directly south to the Village in a pedestrian friendly and safe environment.</li> </ul>
<ul style="list-style-type: none"> <li>With respect to residential, the project should be at the current density with up to a 15% increase in density provided there is a mix of dwelling types.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The plan maintains the current residential number of 276 dwelling units.</li> </ul>
<ul style="list-style-type: none"> <li>A mix of housing types that preserves the potential for retaining rental housing opportunities near the Village is encouraged.</li> </ul>	✓	<ul style="list-style-type: none"> <li>The residential apartments are contemplated to be for rent.</li> </ul>
<ul style="list-style-type: none"> <li>Enhanced streetscapes and primary building frontages (architectural detail and ornamental entrances) should be established along Lane Park Road to strengthen connections to the Village.</li> </ul>	✓	<ul style="list-style-type: none"> <li>Building frontages will exist along Lane Parke Road with enhanced streetscapes (sidewalks, landscaping, planters, benches). The Inn will have its primary frontage on Lane Park Road. The plan contemplates key tenants on the Lane Parke Road corners. Entrances from Lane Park Road will include architectural detail and ornamentation.</li> </ul>



- ❖ “No buildings shall exceed six stories, or 72 feet to the eave.”
- ❖ “Create significant green space...”
- ❖ “...Discrete and small parking areas. Structured parking away from important streetscapes.”
- ❖ “...Strong connections into the Village...”
- ❖ “Retail uses should remain focused toward Culver.”



- ❖ “Provide at least one stub connection to the North.”
- ❖ “Internal streets should have enhanced pedestrian access...”
- ❖ “(Residential) density should be at the approximate current density.”
- ❖ “Rental housing opportunities near the Village are encouraged”
- ❖ “Enhanced streetscapes and primary frontages on Lane Park”





Retail at Lane Park Road and Jemison Lane



Inn and Restaurant



View Along Jemison Lane of Village Green

# Restrictions & Constraints

- ◆ SIZE: square footage for each use is limited, overall and by block
- ◆ HEIGHTS of buildings are strictly limited
- ◆ STYLES of the buildings are tightly controlled
- ◆ USES are limited to specific areas within the PUD
- ◆ RESIDENTIAL UNITS are limited to existing number (276)
- ◆ INN ROOMS are strictly limited (100)
- ◆ STREET NETWORK is determined in advance

# Restrictions & Constraints

- ◆ OPEN SPACE REQUIREMENTS are set
- ◆ STREETSCAPES and sidewalks must be consistent with Village
- ◆ TRAFFIC IMPROVEMENTS are mandatory
- ◆ ZOO BRANCH CULVERT must be built
- ◆ SETBACKS are closely controlled
- ◆ PARKING structure location is determined

Parking

Jeff Colvin, AICP

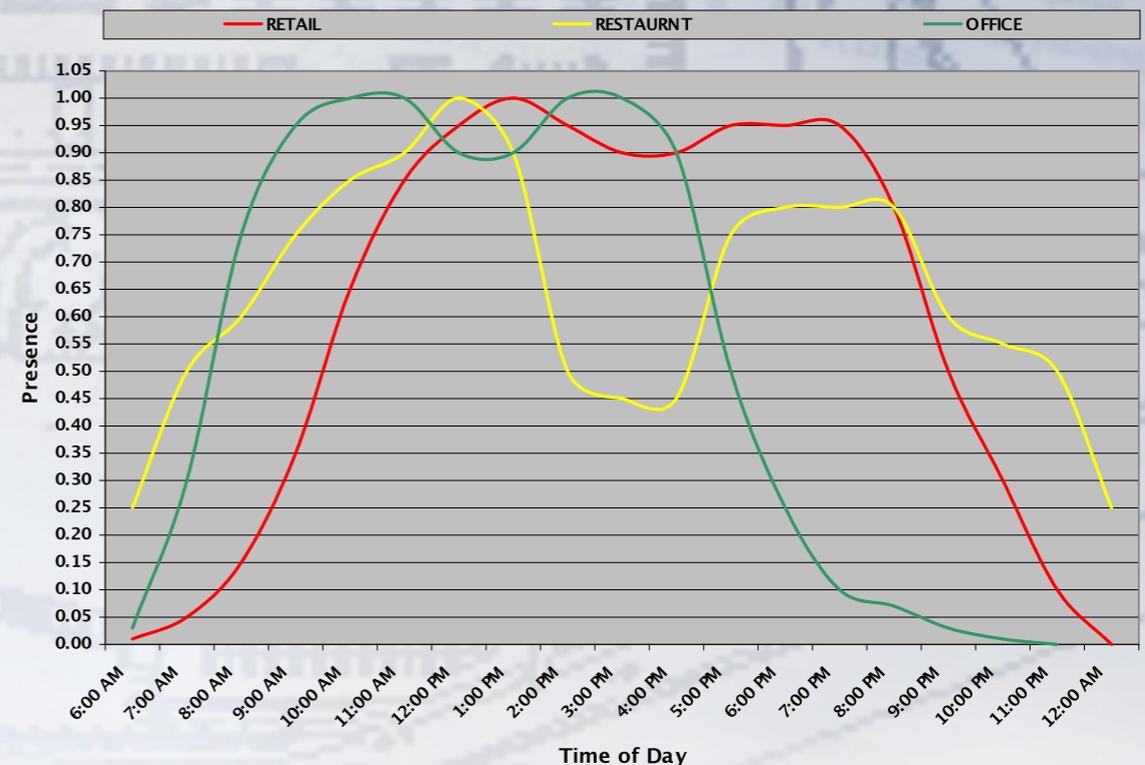
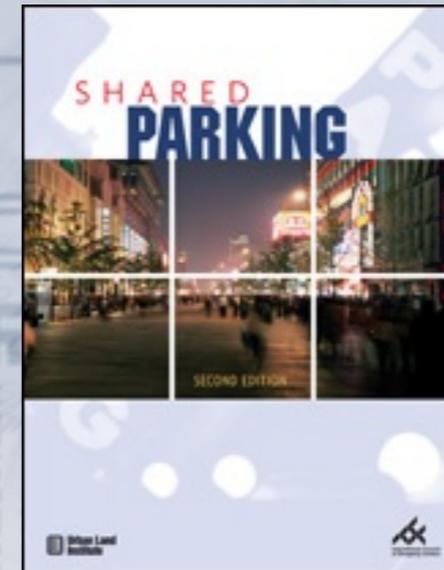
Walker Parking Consultants, LLC

- **The Book**

- Published by Urban Land Institute (ULI) in 2005
- Widely accepted methodology, updated from 1983 publication
- Mary Smith, PE, principal for Walker Parking Consultants, is primary author

- **The Model**

- Unadjusted Demand -- as if stand-alone uses
- Shared Demand -- adjustments for:
  - Month (including a “13<sup>th</sup>” month for the last week of Dec.)
  - Weekday vs. Weekend
  - 6:00 a.m. to Midnight
  - Allows driving and captive market adjustments
  - Calculates peak due to combinations of uses



# Shared Parking Overview

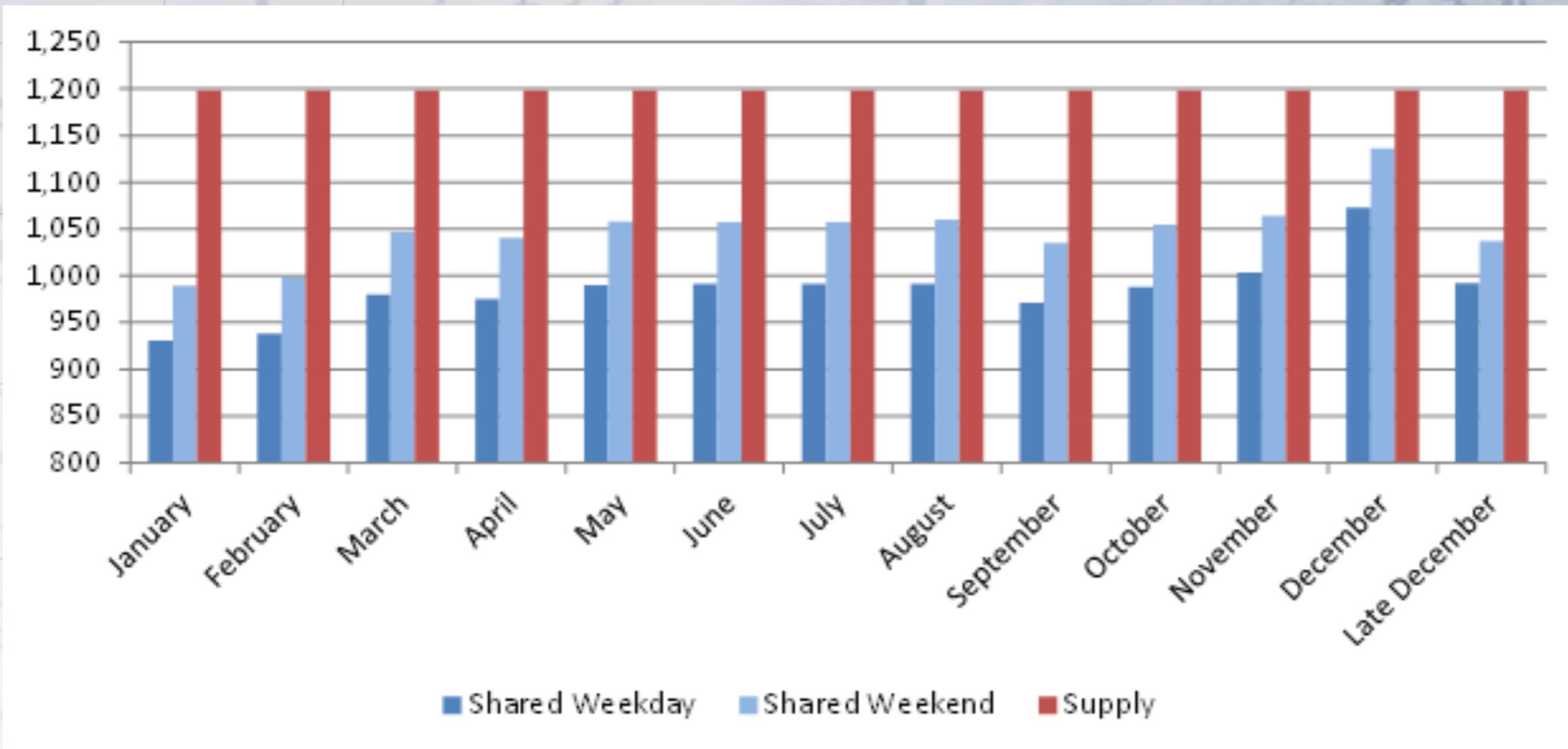


- Typical shared parking reduction of 5-35% from independent demand (31% for Lane Parke)
- Hourly and seasonal demand projections assist in parking management considerations
- Demand projections by user (customer, employee, resident) assist in design and parking management considerations
- Less land required for parking equals more land available for development (and tax dollars)

Land Use	Unadjusted Demand	Month Adj December	Weekend			Demand December 7:00 PM
			Pk Hr Adj 7:00 PM	Non Captive Evening	Drive Ratio Evening	
Retail	302	100%	75%	90%	95%	194
Employee	75	100%	80%	100%	90%	54
Pharmacy	40	100%	100%	95%	95%	36
Employee	10	100%	100%	100%	90%	9
Fine/Casual Dining	230	100%	95%	80%	95%	166
Employee	41	100%	100%	100%	90%	37
Family Restaurant	105	100%	70%	80%	95%	56
Employee	19	100%	95%	100%	90%	16
Fast Food	99	100%	80%	40%	95%	30
Employee	16	100%	90%	100%	90%	13
Hotel-Business	90	67%	75%	100%	77%	35
Employee	18	100%	55%	100%	90%	9
Residential Guest	7	100%	100%	100%	100%	7
Residential Shared, Rental	414	100%	97%	100%	100%	402
Office - 100k sq ft	1	100%	0%	100%	95%	0
Employee	8	100%	0%	100%	90%	0
Bank (Drive In Branch)	14	100%	0%	100%	95%	0
Employee	7	100%	0%	100%	90%	0
Grocery	134	95%	58%	90%	95%	63
Employee	25	100%	40%	100%	90%	9
Subtotal Customer/Guest Spaces	1,022					587
Subtotal Employee Spaces	633					549
Subtotal Reserved Spaces	0					0
Total Parking Spaces	1,655					1,136
<b>Shared Parking Reduction</b>						<b>519</b>
<b>Spaces:</b>						<b>31%</b>
<b>% reduction</b>						

# Benefits of Shared Parking





# Peak Demand by Month



1. Base Code Requirement is determined using the Village Overlay Standards [Article XXXI. Section 129-555. (b) (3)]
2. Uses code as base ratios, then applies shared parking concept
3. Based on ULI's "Shared Parking, Second Edition"

Land Use	Required Parking Ratio	Total	
		Units	Spaces
Retail	5.0 per 1,000 sf	144,265	722
Office	4 per 1,000 sf	25,043	101
Hotel	1 per key	100	100
Residential	2 per du	276	552
Required Supply			1,475
Over/(Short)			1,201
			(274)

## Code Comparison



- The peak parking demand occurs at 7:00 p.m. on the weekend in December
- The projected peak hour demand is 1,136 spaces (1,073 spaces for weekday)
- Lane Parke will provide approximately 1,201 spaces on-site in surface lots and one parking structure. (using valet, that number rises to 1,265)
- During peak conditions, a surplus of about 65 spaces is projected

## Summary



Traffic

Darrell Skipper, PE &  
Richard Caudle, PE  
Skipper Consulting, Inc.

Skipper  
Consulting, Inc.  
*Revised Traffic Study*

TRAFFIC  
IMPACT  
STUDY

Lane Parke

Mountain Brook, Alabama

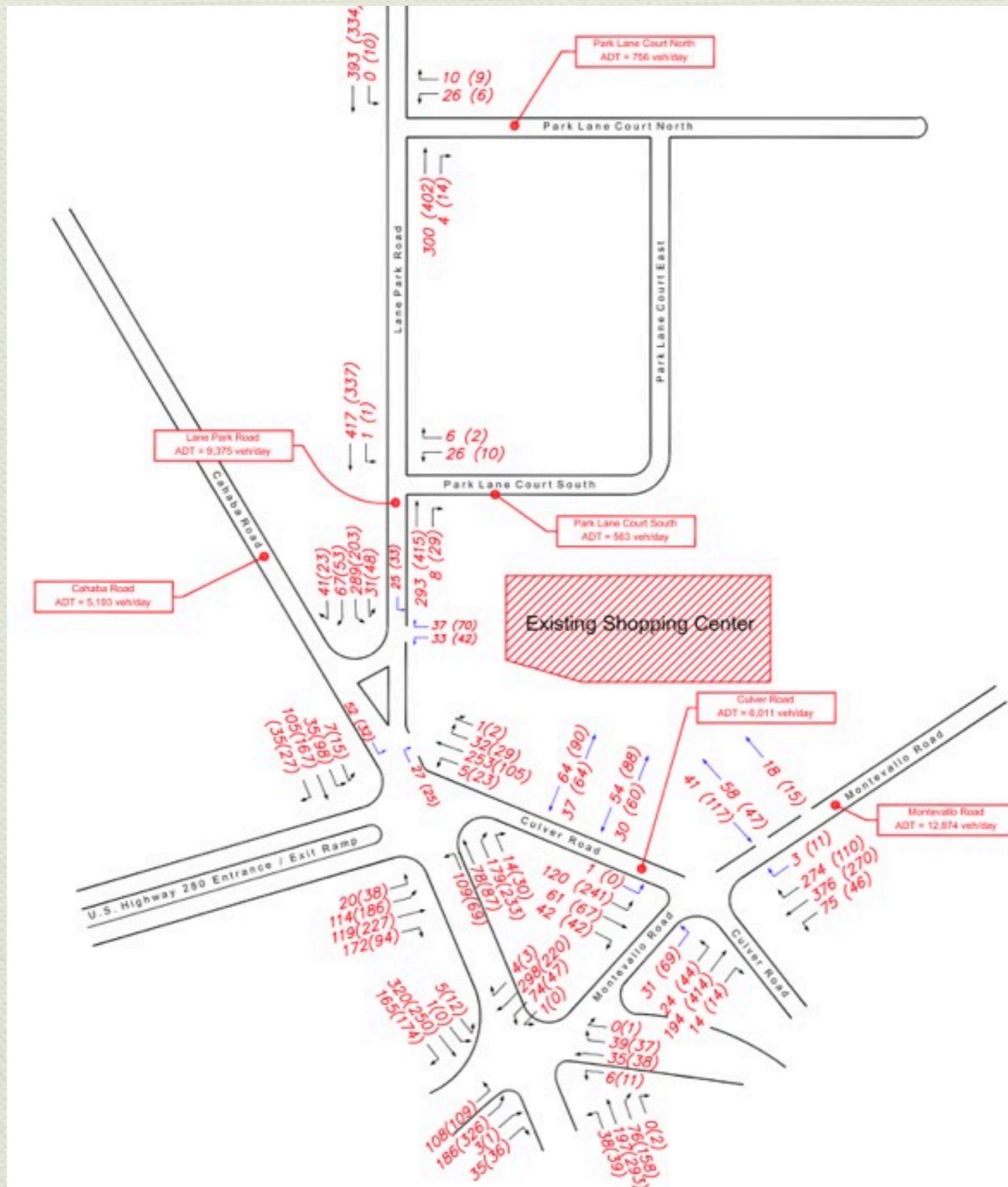


Prepared for:  
GOODWYN, MILLS AND CAWOOD, INC.

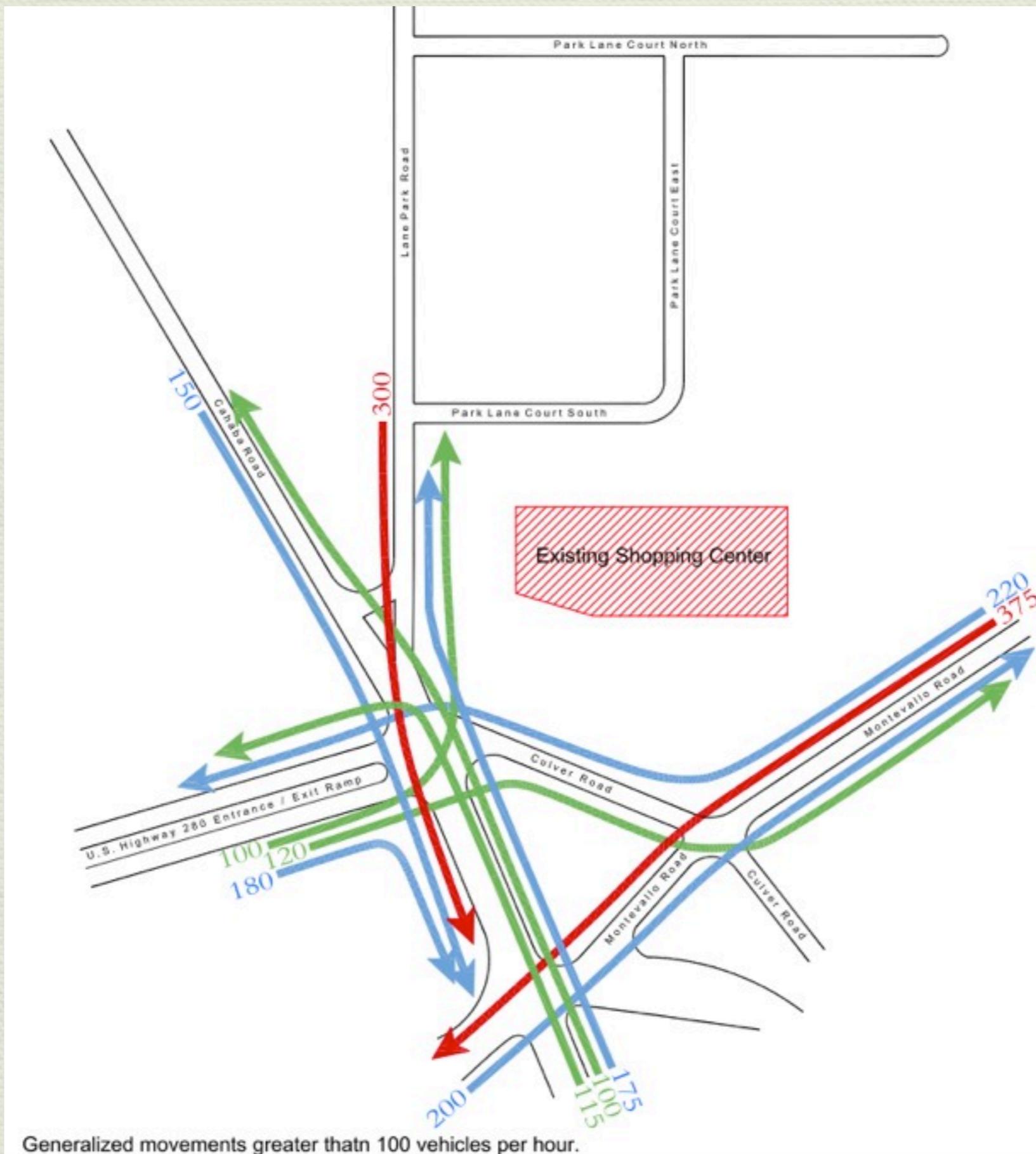
Prepared by:  
**SKIPPER**  
CONSULTING INC.

September 2009  
Revision 1: May 2010  
Revision 2: January 2012

# Existing Traffic Counts

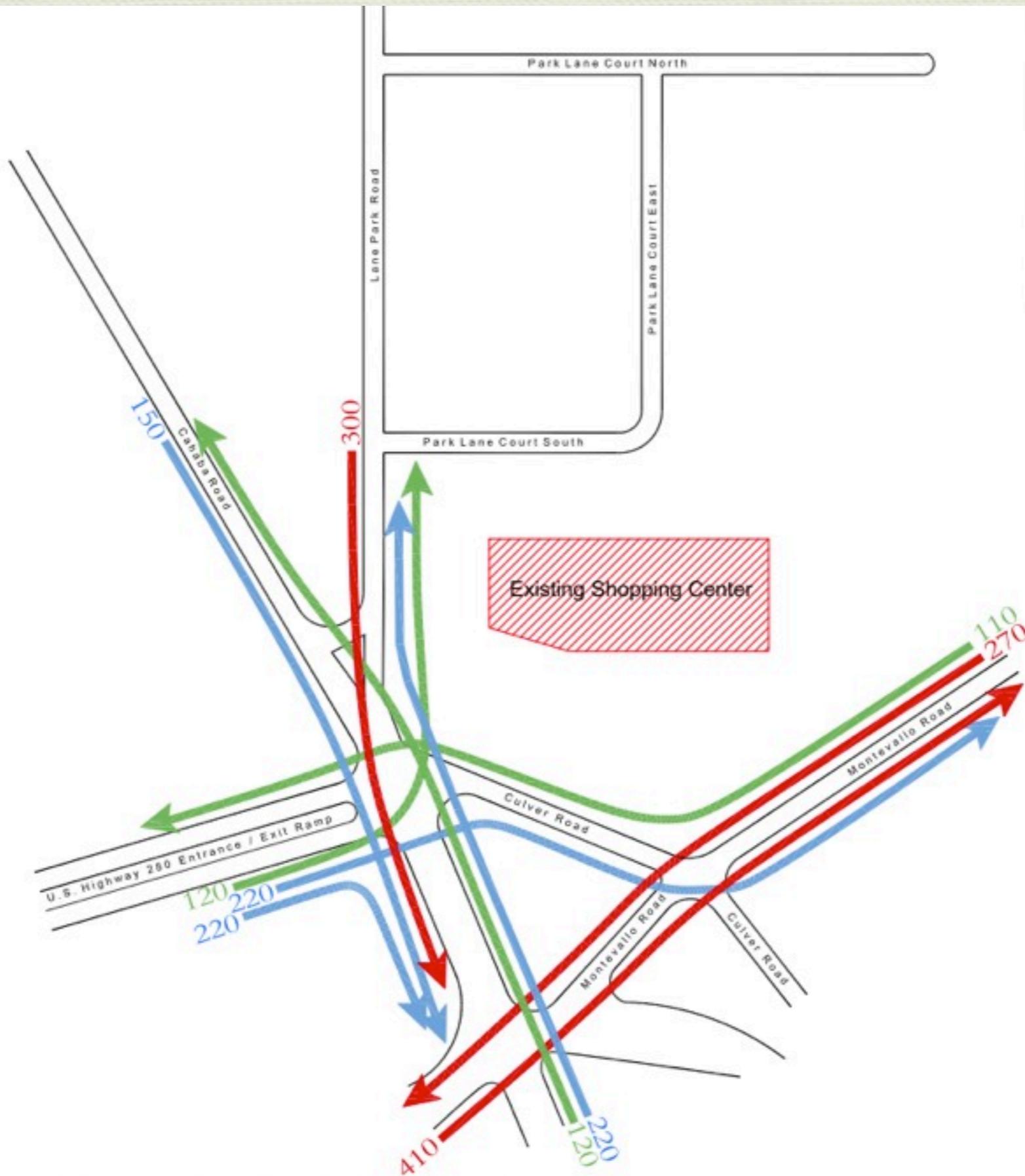


Commuter  
Desire  
Lines - AM  
Peak Hour



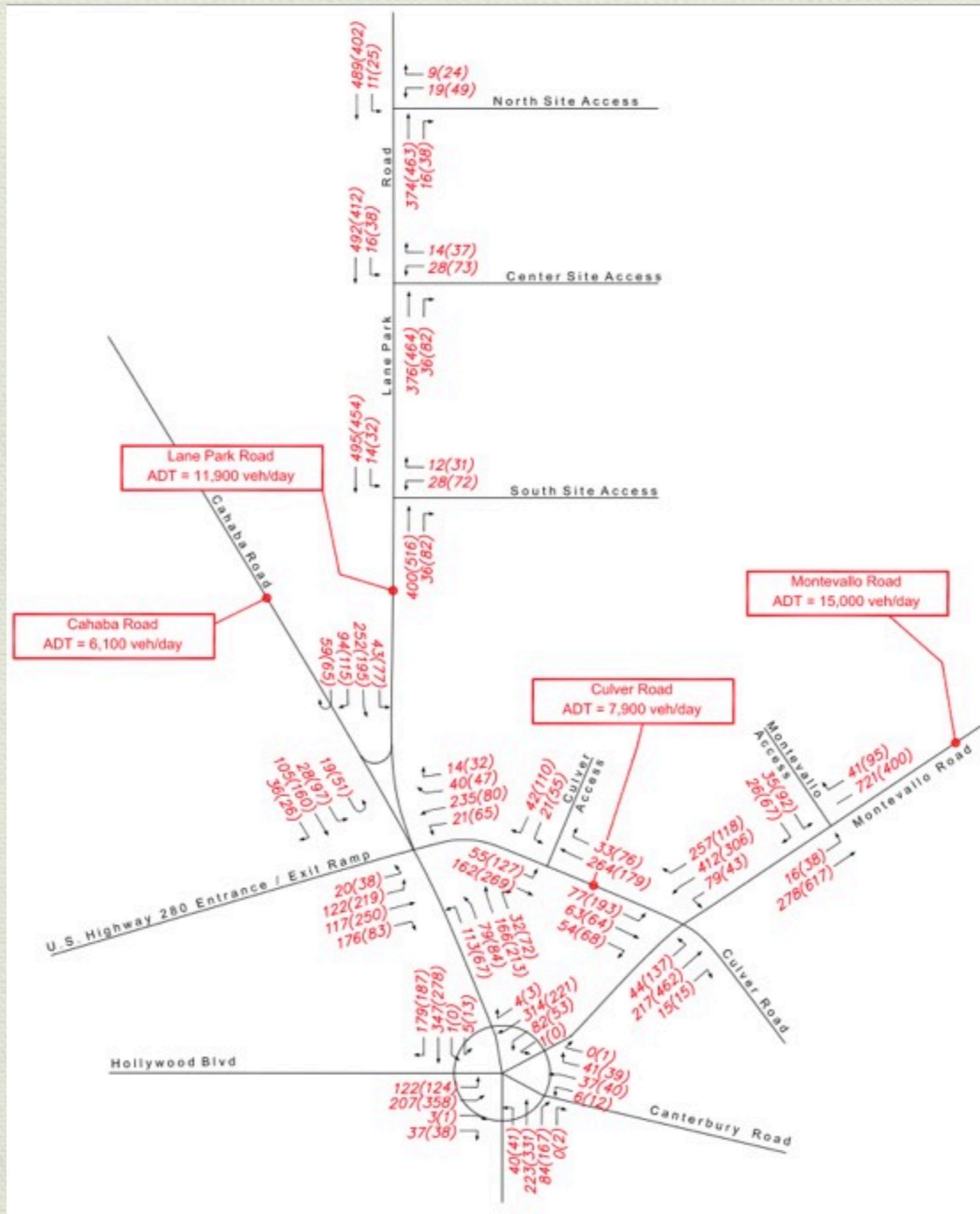
Generalized movements greater than 100 vehicles per hour.

Commuter  
Desire  
Lines - PM  
Peak Hour



Generalized movements greater than 100 vehicles per hour.

# Future Traffic Volumes



# Recommended Access Schematic



# Pattern Book

# Domenick Treschitta, RA

## Historical Concepts

# PATTERN BOOK

GENERAL DESIGN GUIDELINES  
& REGULATING PLAN



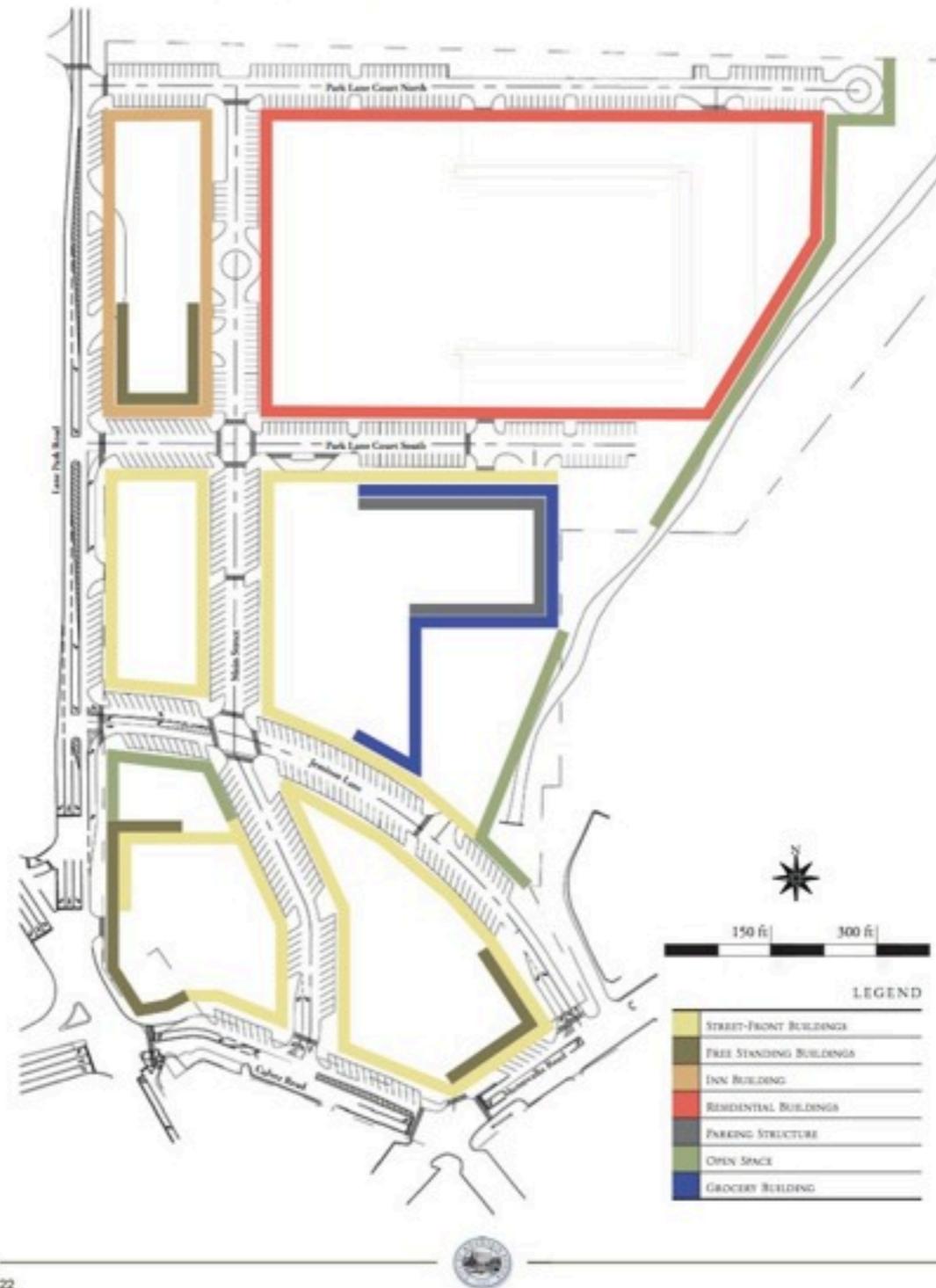
BUILDING TYPOLOGIES



ARCHITECTURAL STYLES

## DESIGN STANDARDS & PATTERN BOOK

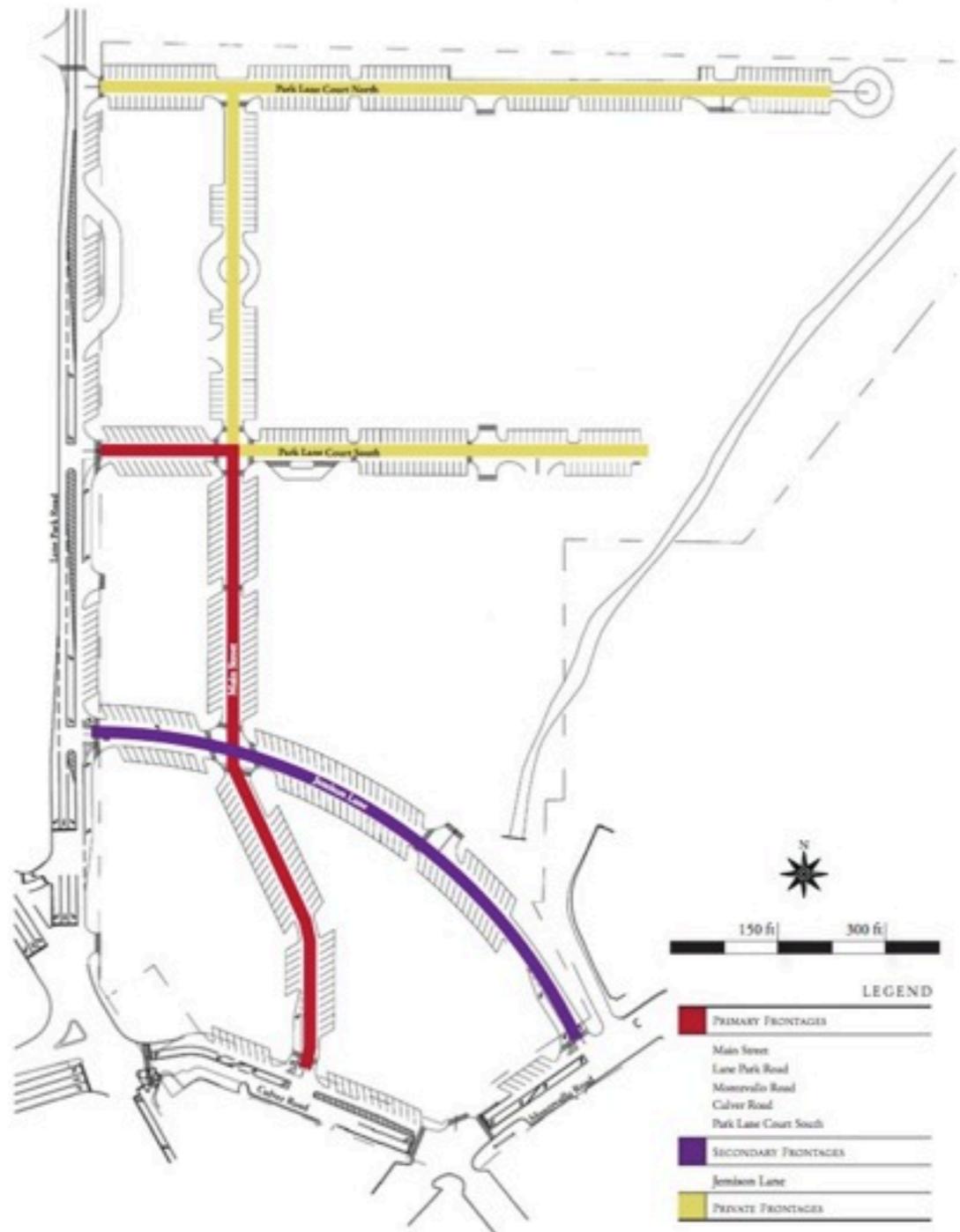
REGULATING PLAN — BUILDING TYPOLOGIES



LEGEND

Yellow	STREET-FRONT BUILDINGS
Green	FREE STANDING BUILDINGS
Orange	INN BUILDING
Red	RESIDENTIAL BUILDINGS
Grey	PARKING STRUCTURE
Light Green	OPEN SPACE
Blue	GROCERY BUILDING





STREET FRONT BUILDINGS



FREE-STANDING BUILDINGS



RESIDENTIAL BUILDINGS (NEW)



INN BUILDINGS (NEW)



PARKING STRUCTURES (NEW)



GROCERY BUILDINGS





The pedestrian scale and unique character of the existing buildings and architecture in the Village are key to its continued viability. The Street-Front Building Typology provided in this Section has been developed to maintain this pedestrian scale and unique architecture while allowing for a vibrant and exciting shopping experience unique to the area. The Street-Front Building Typology for Lane Park is based upon the Street-Front Building Type established by the Village Overlay Standards.

The following standards allow for differences in height, massing, scale and materials that are critical in creating the sense that Lane Park is an extension of the existing Village that has evolved over time. Each store space or building's relationship to public streets, open spaces and surrounding architecture should be taken into consideration as material and massing decisions are made to ensure each elevation is sympathetic with the structures and spaces around it.

Street-Front Buildings are those with immediate frontage on streets. To remain consistent with the Village, this Building Typology is low-scale (1 to 2 1/2 stories) with engaging street-level storefronts. The massing, materials and overall design of a Street-Front Building is especially important at street level where a high level of pedestrian activity will take place.

The following pages detail height, massing and orientation standards that govern Street-Front Buildings to ensure a quality and character consistent with the existing structures in the Village. Street-Front Buildings shall meet the following building standards, which standards shall control over any conflicting provisions of the Village Overlay Standards or of the Local Business District Standards.

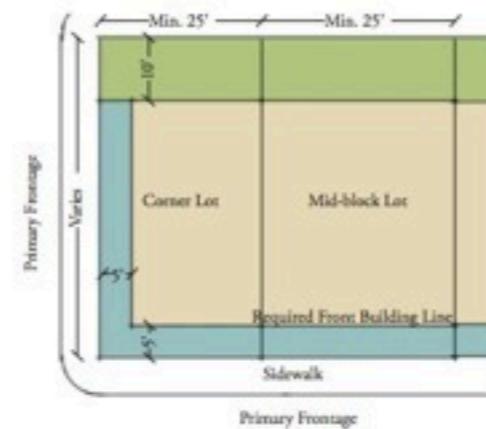


LEGEND

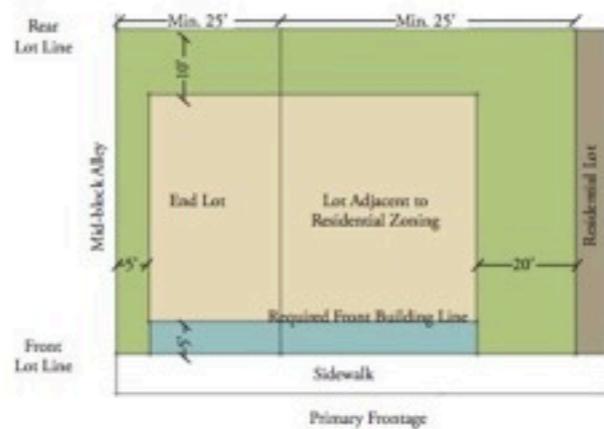
	BUILD-TO ZONE
	SETBACK
	ALLOWABLE BUILDING FOOTPRINT
	RESIDENTIAL LOT
	BUILDING FOOTPRINT

LOT LAYOUT NOTES:

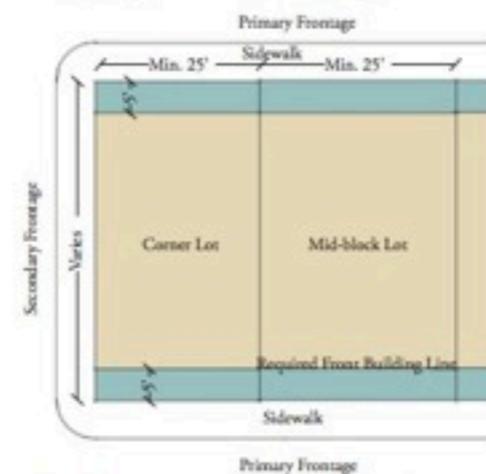
- There is no minimum or maximum required lot depth.
- There is no maximum required lot width.
- The front facade of the building must occur in the Build-To Zone.
- Exceptions to the Street Wall Coverage requirements may be permitted to comply with the Open Space Requirements.
- Maximum lot coverage is 99% of the buildable lot area. Any open space required on the lot shall reduce the maximum lot coverage.
- All references to a lot or depictions of a lot line shall refer to a land condominium Unit or the boundary line of a land condominium Unit, or if no applicable land condominium Unit has been established, then to a Parcel or the boundary line of such Parcel.
- Lot references shall not refer to a condominium Unit other than a land condominium Unit.



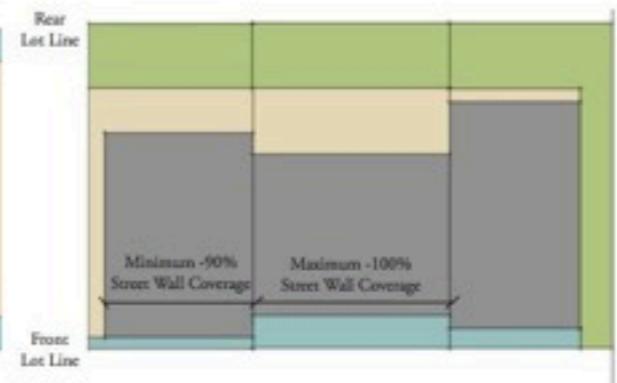
Primary Frontage Corner and Mid-block Lot Configurations



End Lot Configurations



Dual Primary and Secondary Frontage Corner Lot Configuration



Street Wall Coverage Requirements



STREET-FRONT BUILDINGS

MASSING NOTES

NOTES

**Transparency**

- On the facade fronting the primary street, hereafter called the "Primary Facade", the first story shall have a transparency of 60% to 90% of the facade.
- On the Primary Facade, each upper story shall have a transparency of 40% to 70% of the facade.
- On facades other than the Primary Facade that are visible from a public right of way, both the first story and upper stories shall have a minimum transparency of 40%.
- Bay windows and balconies may extend up to 3' over the front building line on upper stories, and shall extend a minimum of 3' and a maximum of 6' from the building facade. The use of loggias and arcades along the ground story of the building is encouraged as an amenity for pedestrians.
- A main entrance is required at a minimum of every 50' on the Primary Facade. Buildings or store spaces of over 4,000 square feet shall have at least one entrance per facade that fronts onto a public street, but are exempted from the 50' requirement.
- Corner buildings shall be considered to have two frontages, but may designate one frontage as the principal frontage that meets all of the standards of this section. The other frontage shall meet all of the standards of this section for at least the first 25 feet of building facade, and shall maintain the required front building line for at least the first 40' of the side frontage.

**Pitched Roof Massing**

- One story buildings with a pitched roof shall have a maximum cornice/ eaves height of 18'.
- Two story buildings with a pitched roof shall have a maximum cornice/ eaves height of 26'.
- Pitched roofs must not extend more than 10' above the eaves if the roof pitch is 5:12 to 13:12.
- Pitched roofs must not extend more than 10' above if the roof pitch is 14:12 to 20:12. Pitched roofs sloped greater than 20:12 will not be allowed.
- No building shall exceed 42' in height.
- Pitched roof structures may contain additional floor area which may be occupied without counting towards the story maximum for purposes of the Regulating Plan, provided any additional floor area is associated with and accessory to the floor area of the inferior story. In this condition, the maximum cornice height may be exceeded by 3', provided that a transparency of 15% to 45% is provided for the half-story through the use of dormers.

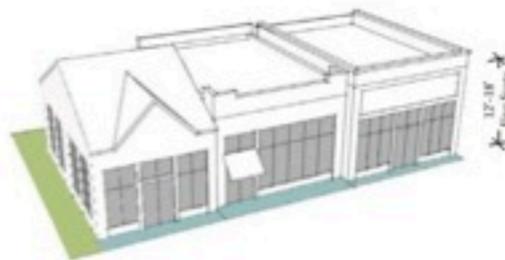
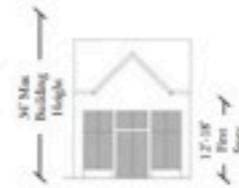
**Flat Roof Massing**

- Parapets must extend a minimum of 3' above the top of the roof structure. This minimum height is intended to ensure that all rooftop equipment is hidden from public view.
- Parapets must occur within the maximum building height.
- Buildings or store spaces with a flat roof and parapet are not required to have a cornice/eaves line distinct from the top of the parapet.
- All rooftop equipment shall fall within the permissible roof heights, be located away from slopes or areas exposed to the public street, and otherwise be screened from view from adjacent public streets or be incorporated into the skin of the building or internal to the block.

**Bay Rhythm**

- Differentiated bays should be expressed on each facade of a building or store space directly fronting a public space or street.
- Bays shall be a minimum of 20' and a maximum of 30' wide on Primary Frontages. On any facade that is visible from a public right of way that is directly adjacent to an area of Primary Frontage, at least one bay shall be articulated on the corner adjacent to the Primary Facade. Where these facades are over 60' in length, they must have architectural articulation, such as bays or pilasters, for at least 20% of the facade in addition to the first bay adjacent to the Primary Facade.

ONE STORY BUILDINGS



17'-18' First Story



MASSING DIAGRAMS

STREET-FRONT BUILDINGS

ONE-AND-A-HALF STORY BUILDINGS



TWO STORY BUILDINGS





# Lane Parke

## ARCHITECTURAL STYLES

Each building or façade within Lane Parke shall be designed in accordance with one or more of the following Architectural Styles: (i) Village Romantic, (ii) Southern Mercantile, (iii) Birmingham Classic and (iv) Village Mid-Century. The design of each building, façade, store space or residential space shall conform to the guidelines established herein for the designated Architectural Style, including the Identifying Features and Design Concepts, which must be approved in accordance with the Design Review Process. Multiple Architectural Styles may be applied to the following Building Typologies: (i) Street-Front Buildings, (ii) Free-Standing Buildings, (iii) Residential Buildings, (iv) Inn Building, (v) a Parking Structure, and (vi) Grocery Building. For each Architectural Style, example photographs and illustrations have been included as reference material, provided however, such Design Examples shall not be construed as final designs, nor shall such Design Examples be construed to require the incorporation of any particular design element represented therein. Final designs may vary from such Design Examples provided such designs substantially conform to the guidelines established for the designated Architectural Style. The standards for each Architectural Style shall control over any conflicting provisions of the Village Overlay Standards, the Local Business District Standards or the Residence G Standards.



The VILLAGE ROMANTIC Architecture Style is based on the extensive palette of existing precedent within Mountain Brook Village and the larger Shades Valley area. Mountain Brook and the surrounding villages were developed during the early twentieth-century when the turn-of-the-century romantic movements influenced many architects, planners, and developers. The ultimate source of these movements is the reverence of the traditional European village and its values expressed in quality, natural building materials and a rich variety of simple massed forms.

Roofs are steeply pitched, typically 11:12 to 20:12, and front facing gables are very common and encouraged individually or in groups. Walls are masonry or stucco with half-timbering, stucco, shingle, or siding as upper story surfacing of in gable infill, often beginning over the window head trim. Windows are grouped vertically and horizontally to create larger glazed openings, with the upper sash of all windows being divided into smaller panes over a single pane lower sash. Turrets, arches, bays, brackets, dormers, and chimneys all embellish and add character to Village Romantic structures.

### APPROPRIATE BUILDING TYPOLOGIES

- Street-Front Buildings
- Free Standing
- Residential
- Inn

### IDENTIFYING FEATURES

1. STEEPLY PITCHED ROOFS (11/12 TO 20/12)
2. PICTURESQUE MASSING, OFTEN ASYMMETRICAL IN OVERALL COMPOSITION
3. SECOND STORY OR OTHER KEY ELEMENTS PROJECT FROM MAIN WALL PLANE
4. MASONRY WALLS ON GROUND LEVEL OF MULTI-STORY BUILDINGS
5. POLYGONAL TURRETS OR TOWERS AT INSIDE CORNERS
6. HALF-TIMBERED WALL SURFACE TREATMENT

### APPROPRIATE BUILDING MATERIALS

- Wall materials: brick, painted brick, stucco, and stone masonry
- Trim/accents materials: brick, cutstone, cast stone, and wood
- Roofing materials: slate, flat clay or concrete tiles, metal or composite architectural shingles

### ADDITIONAL RESOURCES

- *Design in Birmingham* edited by Philip Morris and Marjorie White, published by the Birmingham Historical Society.
- *Mountain Brook Village: Then & Now* by Linda Nelson and Marjorie White, published by the Birmingham Historical Society.
- *The Abrams Guide to American House Styles* by William Morgan.
- *A Field Guide to American Homes* by Virginia and Lee McAlester.
- *American Homes: A Field Guide* by Gerald Foster





VILLAGE ROMANTIC DESIGN CONCEPTS

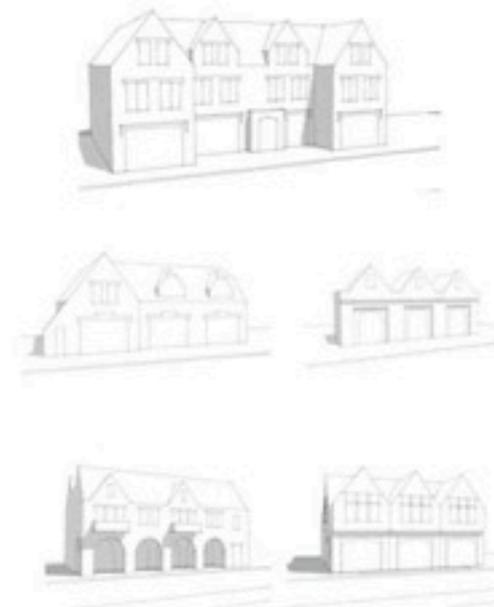
**SYMMETRY** - A Village Romantic building will have a picturesque structural order guided by the eye for an overall harmonious composition. Bilateral building symmetry is not common and is discouraged. Individual design elements or groups of elements can be symmetrical to each other however. While this apparent contradiction makes the style more difficult to prescribe than others, the results of a well composed Village Romantic building will have a timeless, charming character.

**PROPORTION** - All historic styles are grounded in simplicity of proportion formulas, if any are present at all. Many times proportion is simply that which "looks" right. In the Village Romantic style, 1:2, 1:3, 2:3, and 3:5 are key proportional relationships to consider. Wall opening dimensions need to honor standard masonry unit sizes to minimize awkward material cuts and misaligned openings. The overall goal of the style should be an appearance of modest, durable charm.

**SURFACE** - A Village Romantic surface is meant to be textured. Wall material should be as natural as possible, brick, stone, or wood, with natural undulations across the masonry faces to create soft variations in shade and shadow. Openings are recessed deeply to create strong shadow lines and trim profiles are projected to unify grouped elements.

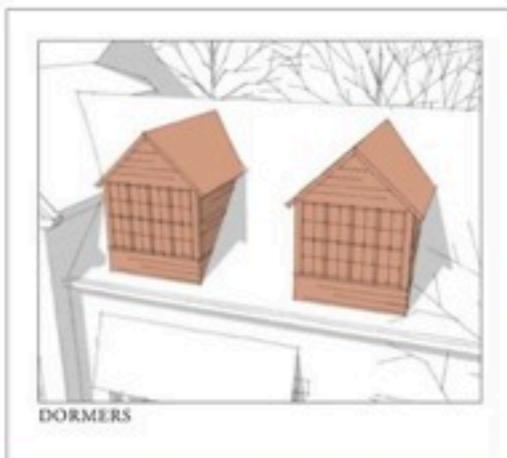
**HIERARCHY OF SCALE** - The scale of details are very important in this style to be appropriate to the building material being used. Stone and masonry details will be larger and more simple than those of details constructed in wood. Details are more prevalent in this style more so than any others because of the variety of architectural elements at your disposal; it is better to be reserved and get a few great details versus ambitious and design a lot of mediocre or incorrect ones. Monumental design elements to be viewed from afar are not typical due to the inherent modesty of the architectural style.

VILLAGE ROMANTIC MASSING EXAMPLES



DESIGN ELEMENTS

VILLAGE ROMANTIC



Village Romantic Dormers are a very important roof element in this style to create visual interest within the mass of roof that steeper roof pitches create. They will most often be gabled in form, but may also be shed or a clipped gable. They can be located solely on the roof plane or be placed on the wall and provide a break in the eave line.

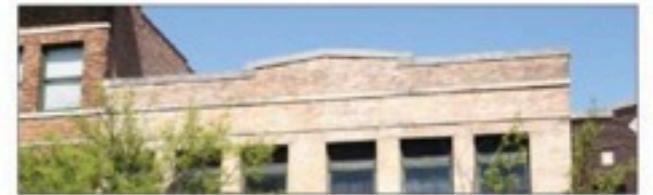
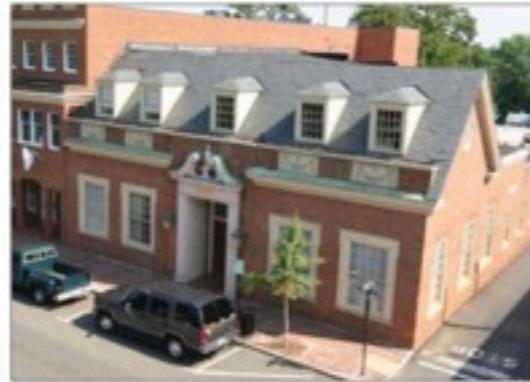
Dormers are typically large, with windows grouped in numbers from 2-4. They can be handled successfully in many different ways, but the most egregious error is for it not to be in proportion to the rest of the roof. Their placement needs to be composed well just as any other building element.



EXAMPLE GALLERY

SOUTHERN MERCANTILE





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